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1946

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340
Engineers

IN THE PACIFIC



U. S. S. R.

CHINA

JAPAN



INDIA

Pa
Oc

PHILIPPINE
ISLANDS

SAN
FABIAN

LUZON

MOROTAI

MAFFIN BAY
HOLLANDIA

FINSCHEFEN

NEW GUINEA

BORNEO

SARAWAK

DARWIN

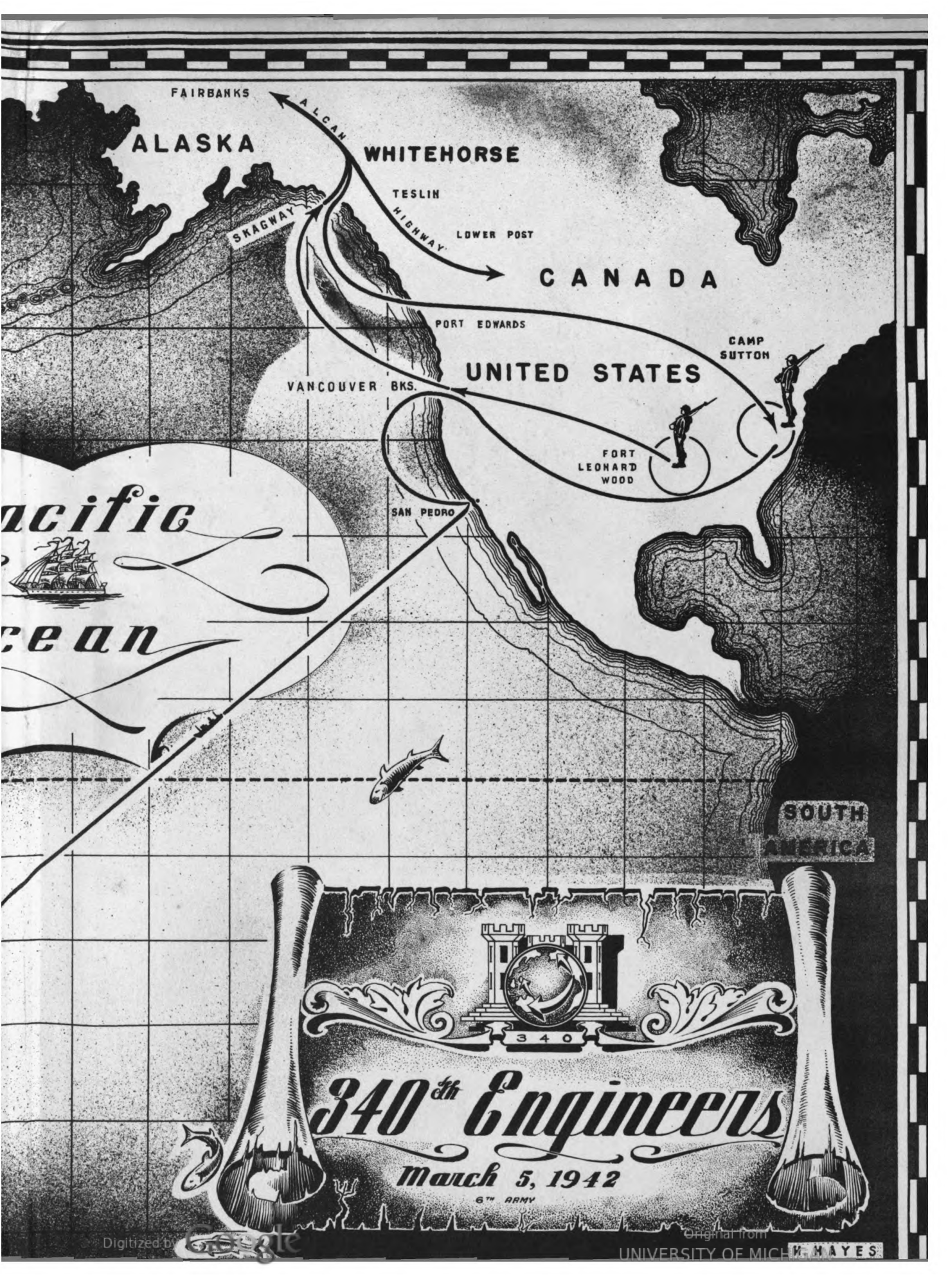
PORT MORESBY
THURSDAY I.

TOWNSVILLE

AUSTRALIA

NEW
ZEALAND





FAIRBANKS

ALASKA

WHITEHORSE

TESLIN

LOWER POST

SKAGWAY

CANADA

PORT EDWARDS

CAMP SUTTON

UNITED STATES

VANCOUVER BKS.

FORT LEONARD WOOD

SAN PEDRO

SOUTH AMERICA

Pacific Ocean



340th Engineers

March 5, 1942

6TH ARMY

7D

1817

AP

1817



A black and white photograph of a tropical beach. In the foreground, several tall palm trees lean over the sand. A low wooden fence runs along the shoreline. In the background, the ocean meets a cloudy sky. Several people are visible on the beach, including a child in the lower right corner.

340TH ENGINEERS IN THE PACIFIC

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Original from
UNIVERSITY OF MICHIGAN



. . . From these honored dead we take increased
devotion to that cause for which they gave the
last full measure of devotion . . .

Abraham Lincoln

JAMIE H. ALEXANDER

ALBION J. CHAISSON

ROBERT CURRAN

JOE B. GONZALES

EUGENE E. HALL

AMEL T. HOCKING

JOSEPH M. KOTZ

THOMAS W. PRICE

GERALD C. SCHMIDT

GARLAND W. STEELE

WILLIAM E. STEPHENSEN

JULIUS ZUPAN





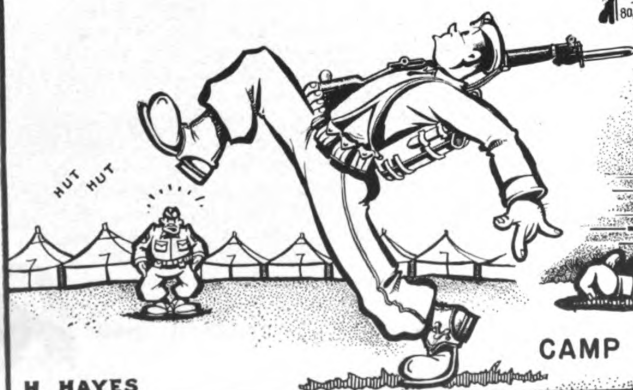
"ROOKIE"



FORT L. WOOD, MO.



"SOURDOUGHS"
ALASKA - CANADA



CAMP SUTTON, N. C.



H. HAYES

GENERAL HISTORY

The following is a history of the 340th Engineers continued from the terminated account given in the previous album which dealt chiefly with Alaska and Canada.

In August, 1943 while it was spread from Whitehorse in the Yukon to Fairbanks, Alaska, the unit was alerted for movement back to the States. The entire regiment departed from Whitehorse on the White Pass and Yukon Railroad, arriving at Skagway, Alaska, on August 16, 1943. Here the men boarded passenger ships of the Canadian lines for a sumptuous voyage to Prince Rupert, British Columbia, Canada. Then after seven days and seven nights on a troop train, the 340th Engineers arrived at Camp Sutton, North Carolina, September 4, 1943, for Stateside duty after 16 months in the Far North.

During the five months at Camp Sutton the regiment trained under the Engineer Unit Training Center; it was here that the personnel of the regiment were indoctrinated and processed to meet overseas requirements.

On the afternoon of February 10, 1944 the regiment left North Carolina enroute to Camp Hatheway, Vancouver Barracks, Washington—five days via troop train. On February 15 the unit arrived at the Camp Hatheway staging area where final preparations were made for the move overseas. For most men of the 340th a staging area was far from a new experience, for they had left for the Yukon Region from the same Post some years before.

The night of February 27, 1944 the outfit moved to Portland, Oregon via truck and embarked at the Portland POE aboard the US Motorship Pennant.

With the entire unit aboard, the Motorship Pennant sailed south along the west coast through a rough sea to San Pedro, California. The ship anchored here one day while tankers pumped fuel into her storage tanks, preparing her for the long voyage. Then, all alone, with no protective escort, she left San Pedro March 5, 1944 at 0900, enroute to the fabled South Pacific War for a secret destination.

The ship continued on a west-by-south course (staying out of sight of all islands) across the Equator and the International Date Line. On March 11 when the Pennant crossed the Equator all Army men were initiated and became members in good standing of the world famous "Shellbacks". March 18 was lost when the ship crossed the Date Line.

During the voyage the Regiment maintained personnel and equipment by daily calisthenics and periodic rifle inspections, respectively. On March 12, 1944, a member of Company B died, and he was buried at sea the following day. Two chaplains officiated at the military burial.

The first sight and touch of land came when the Regiment debarked temporarily at Townsville, Australia on March 24 and hiked to the Armstrong-Paddock Staging Area. Everyone did some bunk-fatigue while orders were changed, sending the outfit to Darwin instead of to Finchhaven, New Guinea.

The unit embarked March 27 on the Pennant to continue its journey, and on April 4, 1944 the 340th arrived at Darwin, Northern Territory, Australia. The men were trucked out to the RAAF Field to bivouac for the night. The ocean trip had taken 37 days.

Immediately the Regiment was put to work on the construction of the RAAF Airdrome taxiways, hangars, and facilities in preparation for its use by a B-29 Bomb Group. The men went to work and continued until just a few days before leaving in August, 1944.

In July the reorganization and redesignation of the 340th Engineer General Service Regiment to the 340th Construction Battalion took place. The excess personnel went to form the newly activated 1180th Engineer Construction Group and the 3014th Engineer Maintenance Company. The physical manifestations of the reorganization took place on the 21st and 22nd of July with a

minimum of confusion. The change naturally resulted in the shuffle of many key officers and men; Lt. Col. J. B. W. Corey, Jr. was given command of 1180th, relinquishing command of the 340th to Major T. M. Maxwell. Col. Corey took with him Majors F. W. Stoltz and H. J. Vanderveer. The 340th and 1180th Sgt.-Majors became W. D. Jimmerson, and R. J. Winkler, respectively. M. J. Mosele went with the 3014th.

August 17th, 1944 the battalion, less two officers and 15 enlisted men, departed from Darwin enroute to Hollandia aboard two Liberty ships, the Don Marquis, and the Sylvestre Escalante. August 26 the ships anchored in the harbor at Hollandia, Dutch New Guinea. It was not until 29th August that the ships docked for unloading. The men then moved by truck 22 miles inland and established camp on Hill 600, in a typical tropical New Guinea downpour. On August 21st the 17 officers and men with the rest of the equipment departed Darwin and arrived on the William Prouse, a Liberty ship, and docked at Hollandia, September 1, 1944. The short time at Hollandia was spent getting ready to go on the Interlude Operation, the invasion of Morotai. Men and equipment of the first group were gathered together in 4 days.

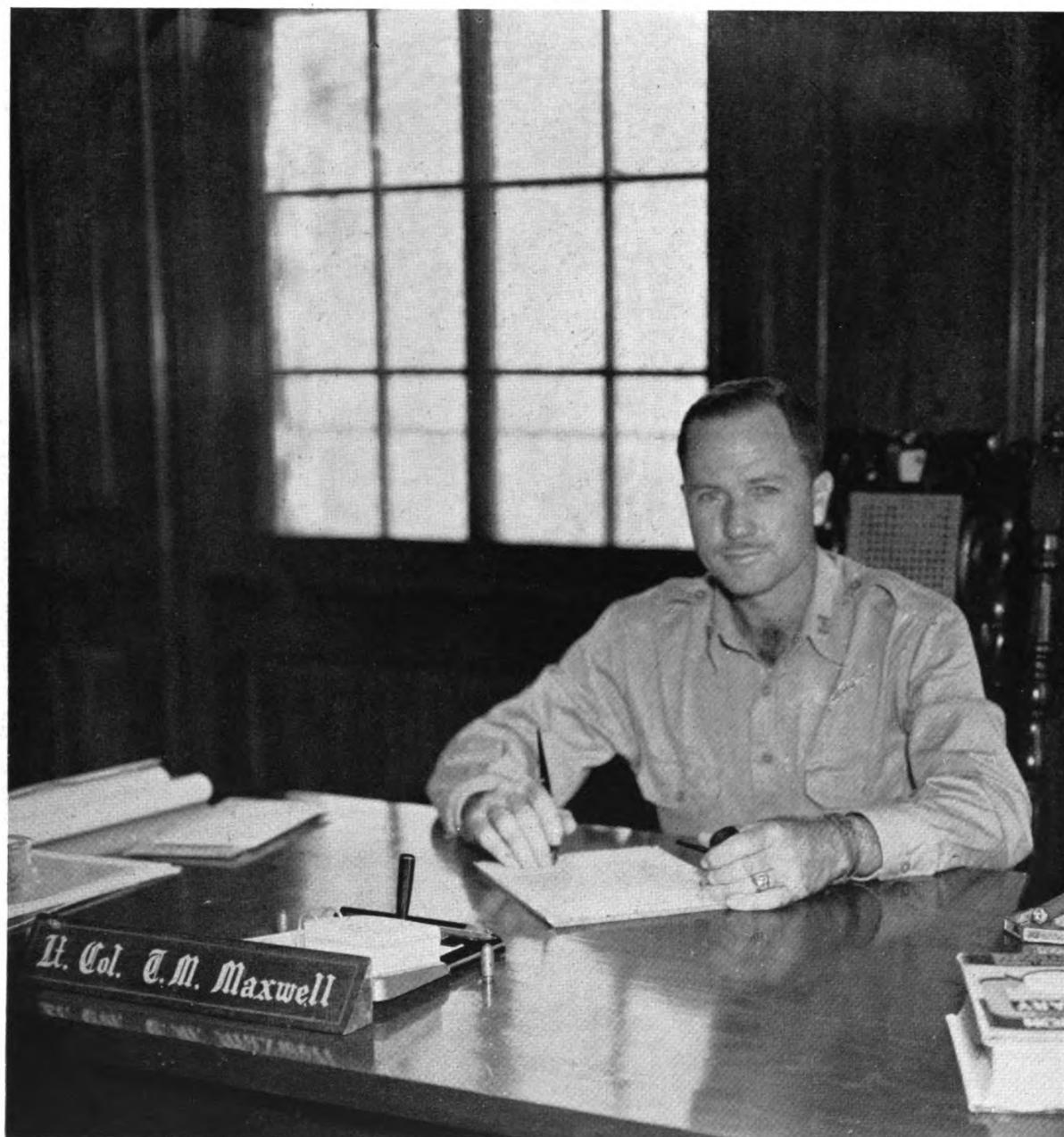
September 4, 1944 ten officers and 290 enlisted men of the 340th departed Hollandia on an LST in the initial convoy, which stopped at Wakde Island for three days to practice landing operations. The convoy arrived at Morotai, Netherlands East Indies on D-Day September 15, 1944. Eleven officers and 386 enlisted men arrived at Morotai September 16. Three smaller groups left for Morotai on 12th, 22nd, and 25th of September, each group arriving six days after departure.

Most of the time the 340th was on Morotai was occupied in road, pier, and building construction. Night life was notable for lack of sleep due to the bombing raids of the Japs, even on Christmas night. During December men and equipment were prepared for the M-1 Operation, the invasion of Luzon in the Philippines. The day after Christmas, 1944 the loading of equipment and personnel on two LST's and one AK ship was completed. The remainder of the month was spent aboard ship, joining the 900-ship convoy and coasting between all the Jap-held islands to Luzon. At dawn on the morning of S-Day, January 9, 1945, the convoy entered Lingayen Gulf.

The unit spent 10 months on the Island building every kind of bridge on every highway north of Manila; roads were repaired, building erected, booby traps removed, airstrips laid out, and Japs hunted and killed. The outfit was on the move all the time until September, 1945, when it prepared to go to Japan. This move was called off, and everyone sweat out October waiting to go home. Finally, in November, most of the eligible men and officers were shipped home, and shortly took up the good civilian life. The 340th was virtually no more by December, 1945, when the last of the low-point men were transferred out, leaving only two men to wait for the last order to the organization, the War Department inactivation order.

The complete story of the 340th Engineers, their experiences, their work, and their travels would fill several volumes. In this book, an attempt has been made to present in picture and story an outline of the adventures of the outfit. The descriptions and photographs that follow are the simple facts of who, what, when, and where, and only begin to tell the whole story. The details, the color, the personal flavor, the humorous trimmings are left to be filled in by the men of the 340th; for who can describe the fear of death, and the sound of shrapnel, the whistle-crunch of bombs, the techniques of building bridges, the midnite Jap-hunting excursions, the joy of coming home—who better than he who has experienced it.

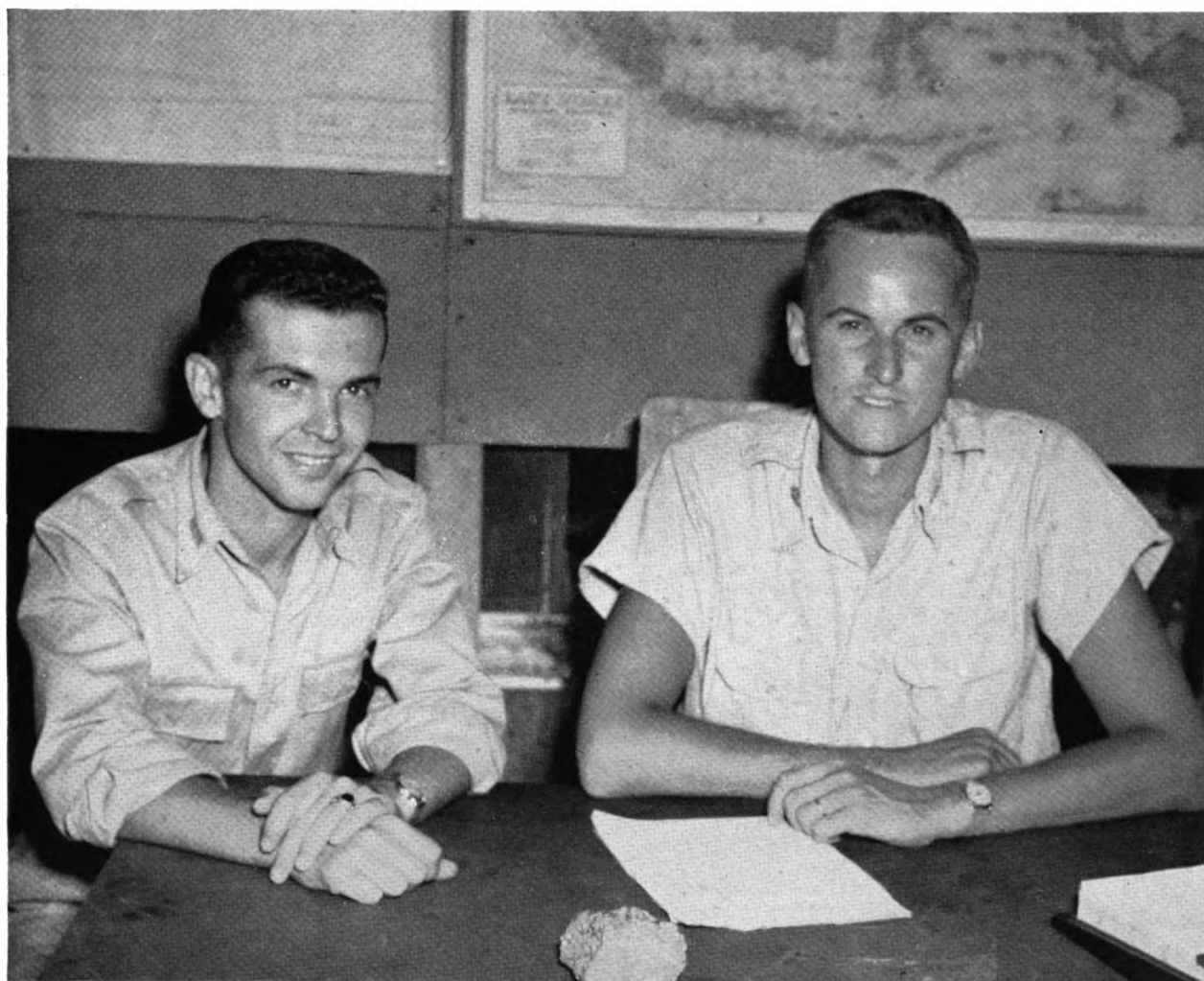
COMMANDING OFFICER



LT. COL. THOMAS M. MAXWELL

Lt. Col. Maxwell was 1st Bn. Commander in the 340th Engr. G. S. Regt. until the Regiment was reorganized to a Construction Battalion; he commanded the Battalion from July, 1944, until October, 1945.

FORMER COMMANDING OFFICER AND EXECUTIVE OFFICER



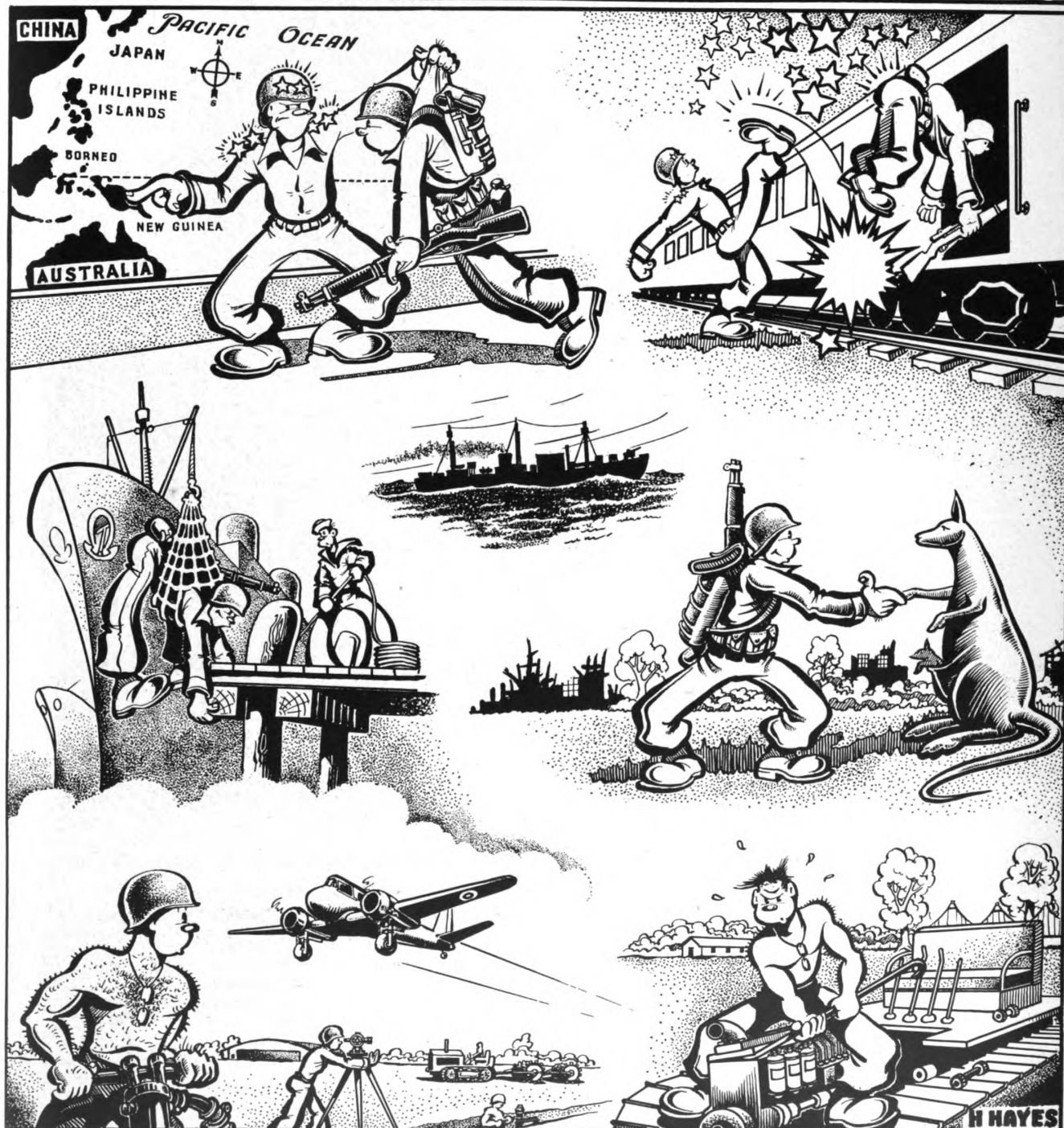
Major William C. Gribble, Jr., and Lt. Col. J. B. W. Corey, Jr.

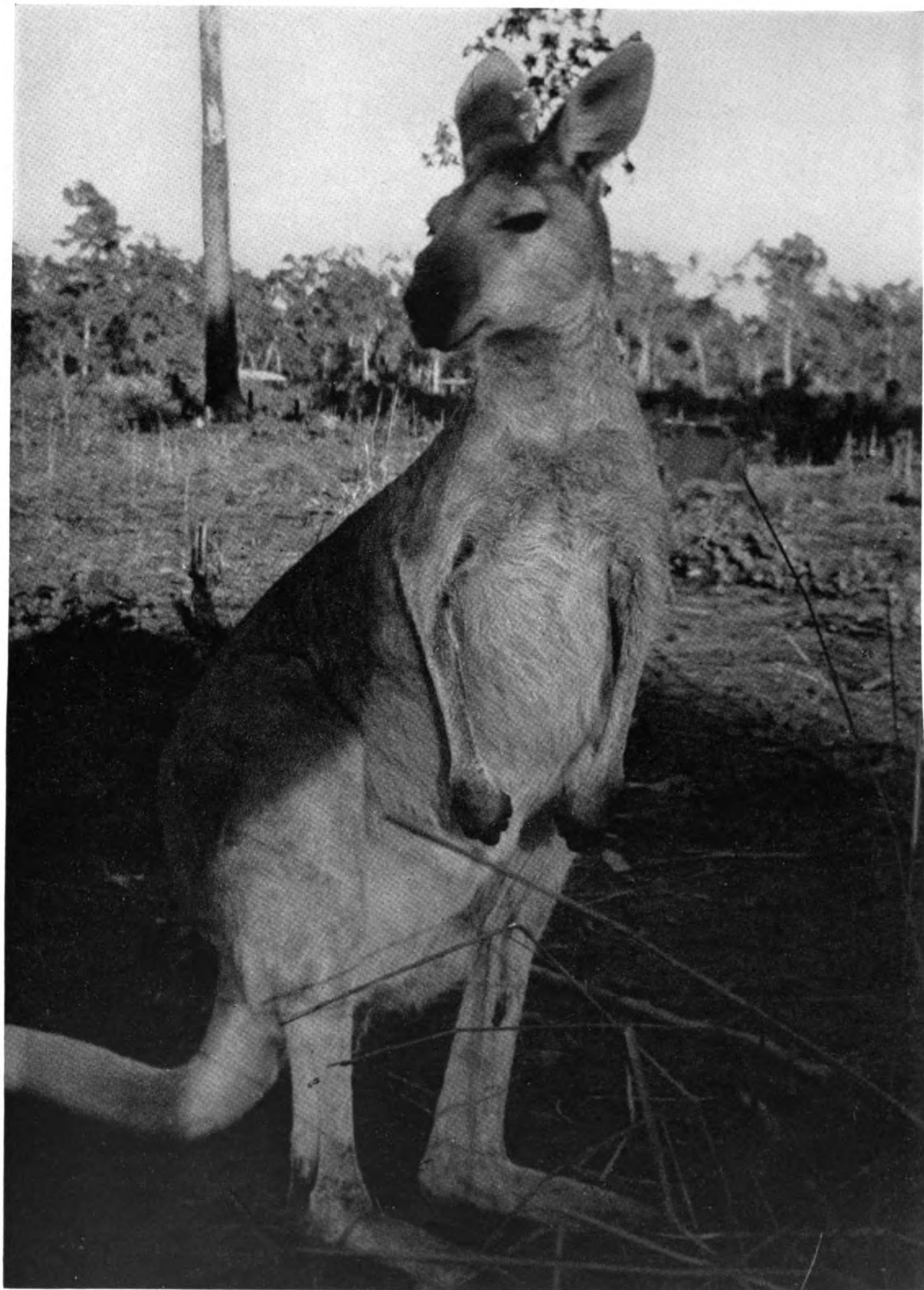
Lt. Col. Corey assumed command of the 340th Engineer Regiment on the Alaskan highway in February, 1943, and piloted the unit through its interesting and often times hectic experiences there, at Camp Sutton, and until the 340th's reorganization at Darwin, July, 1944.

Major Gribble was Executive Officer of the Regiment and remained with the new Construction Battalion in the same capacity until transferred during the Luzon Campaign to the 43rd Infantry Division.

Darwin

Australia







Bank of Darwin located in heart of town.
Damaged by Japanese bombs in 1942.



The Police Station was one of the few undamaged buildings.



Some of the Japs didn't get away.



The Main Drag. Civilians were evacuated in '42.

DARWIN

The swift conquest of the islands of the Dutch Indies by the Jap Armies included Java, Borneo, Celebes, and Timor by May, 1942. The Australian continent was obviously next, and Darwin was most surely the first city which would fall. Darwin was bombed first 14 February 1942 and several other times in February, March, and April. Timor was 400 miles away and invasion seemed imminent. The civilian populace was all removed to southern Australia; the Aussie Army was ordered to pull out of Darwin as soon as the Japs landed and fall back to the southern half of the island. But the Japs never landed.

Darwin's business district and most of the houses were wrecked by the Jap bombs. A few repairs were made on the hotels and banks, but most of the homes were stripped by soldiers. So it may be said that Darwin was almost totally destroyed by the war.

The harbor facilities, the piers, were still intact and were in constant use by U. S. cargo ships and naval vessels. It was an important U. S. submarine base.

The climate is two-faced: "the wet" and "the dry", each of six-months length. During the five months the 340th was there, between April and August, there was only one light sprinkle, yet the annual rainfall is given as 95 inches.

Communication with the rest of Australia is provided by an antiquated narrow-gauge railroad and a winding road. Both are impassable at times during "the wet". The nearest town of any size is 1600 miles away, Townsville.

The remains of the former
U. S. Army Headquarters.
Bombs were not choosey in
their targets.



Remember the long chow
line?—RAAF Mess Hall.



Assets were frozen in the
Bank of Darwin.



In the Botanical Garden.





Overseas F.H.A.



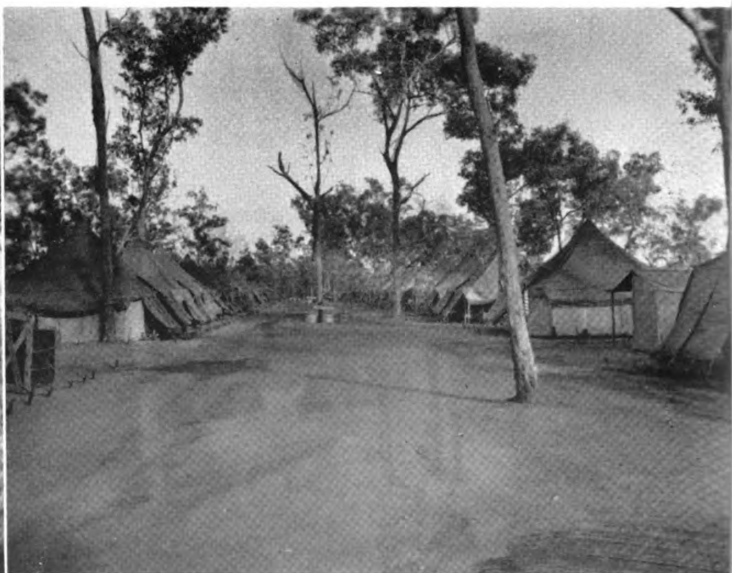
Apartment House Style

First Camp In Australia

All the Modern Conveniences



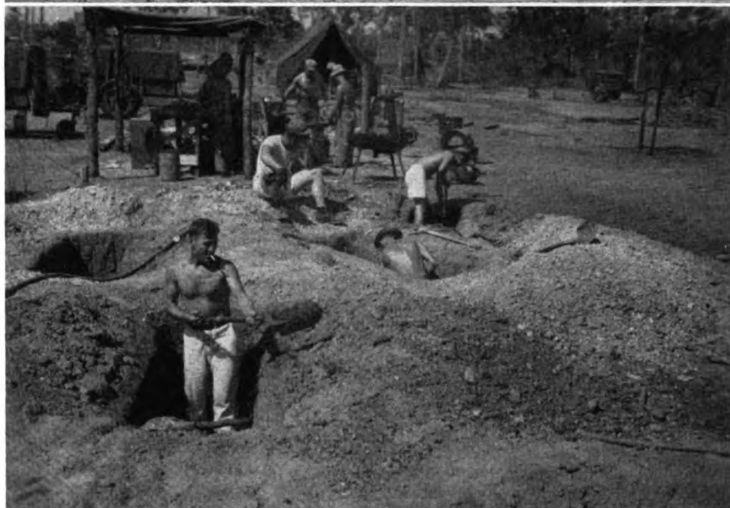
Before



After



Entrance to Regimental Area.



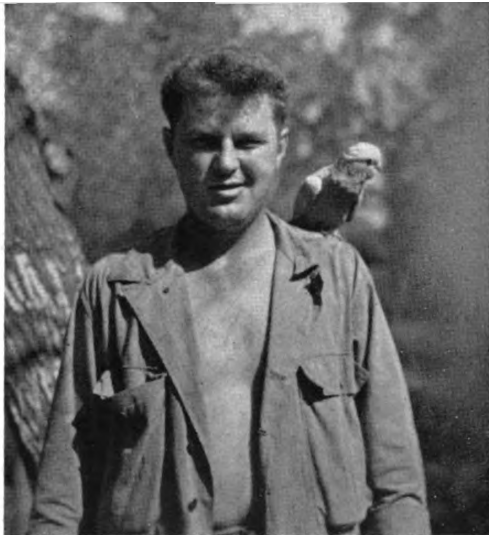
Toth digs deeper after the first air raid.



Company E kitchen—what a mess!



Zito watches Hansen, Olson, and Marshall peeling spuds.



Morgan and his pet cockatoo.



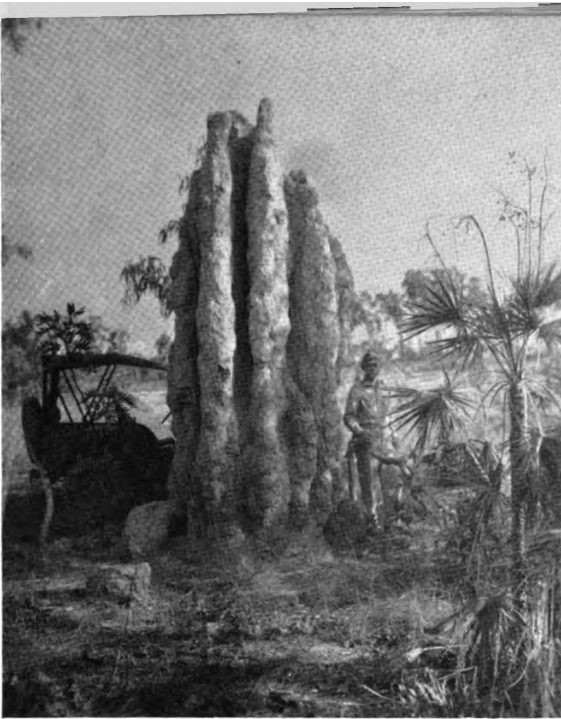
Another bottle baby.



Capt. Petrak and Williams, the tonsorial artist.



The Laundry—or you could wash them yourself.



The Ants built mansions. Lt. Kucera tries it for size.



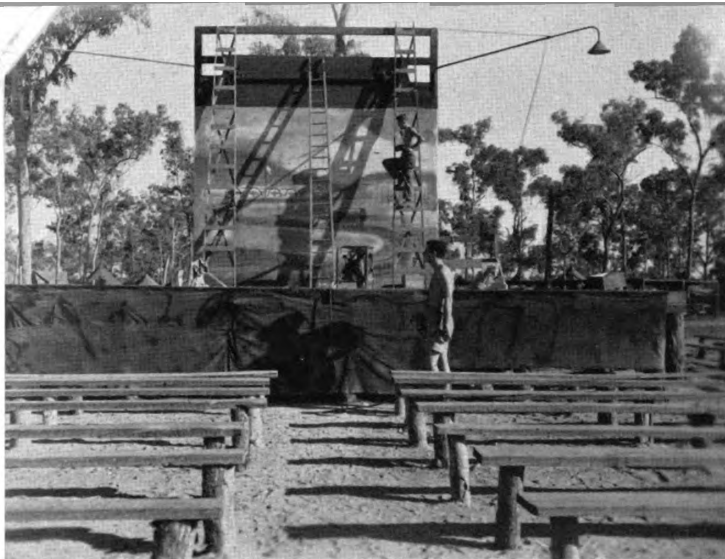
Fussy ants built these magnetic ant hills.



Regimental Water Point.



Regimental Shower. Remember the sun-warmed water? Later it was more private.



Gambling does some mural work on the backdrop while
"Woofle Dust" polices the area.



"Maggie" and Hunt at the mike.



The Darwin Palace Theatre.

Remember Stan Bourne and his Aussie Troop?

Yowzah!



Green and Dee.



Recreation Tent.



"Punchy" Perillo makes connections with
an Aussie.



"Dago" and "Buck" Rogers. Mailloux
referees.





Enlisted staff of Regimental Headquarters. From left to right: Winkler, Scott, Boos, Hazlewood, Yenor, and Sattinger.

Headquarters 340th Engineer General Service Regiment.

Personnel Section looking busy for the picture.



Chaplain Chapple conducts services.



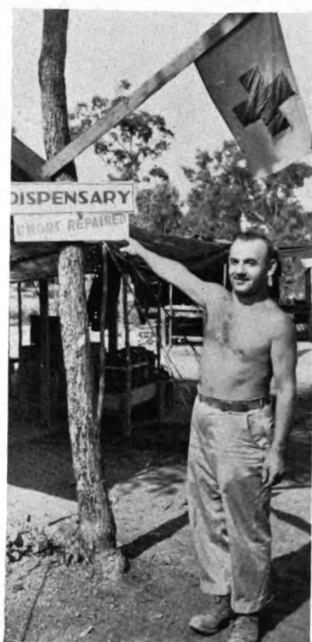
Regimental Review, July 4, 1944. Wallace, Sheahan & Steger lower the Stars and Stripes.



Capt. Wilson and T/5 Crawford prepare to give Bentley the "works".



Regimental "Pill Rollers".



Karl Rath means it too.

Koob, Hatch, Horenberger, & Roy Smith hold down a rock python.

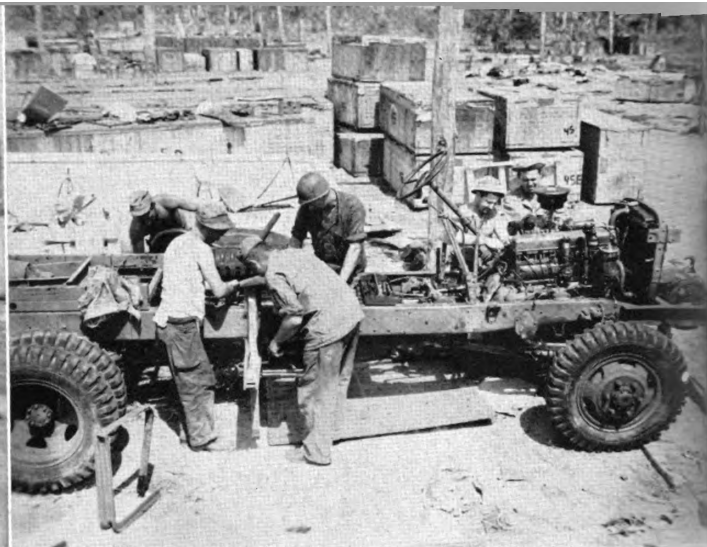


S-3 Field Party.





"Fritz" starts assembling a GMC.



Mooney & crew will have it running soon.

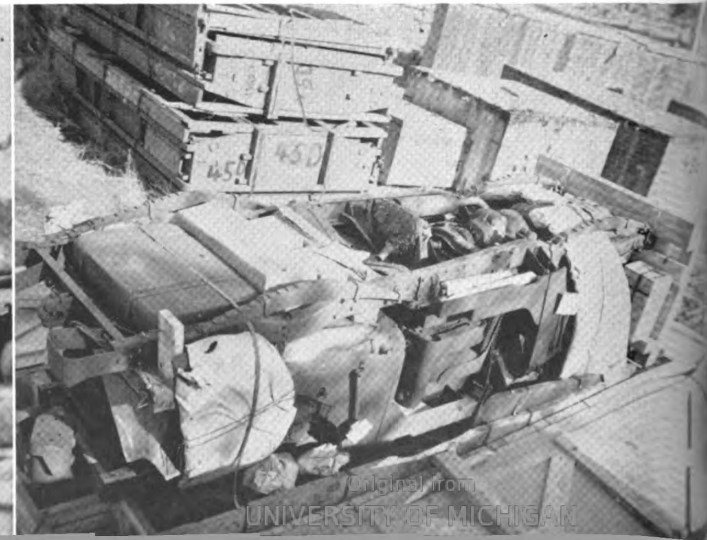
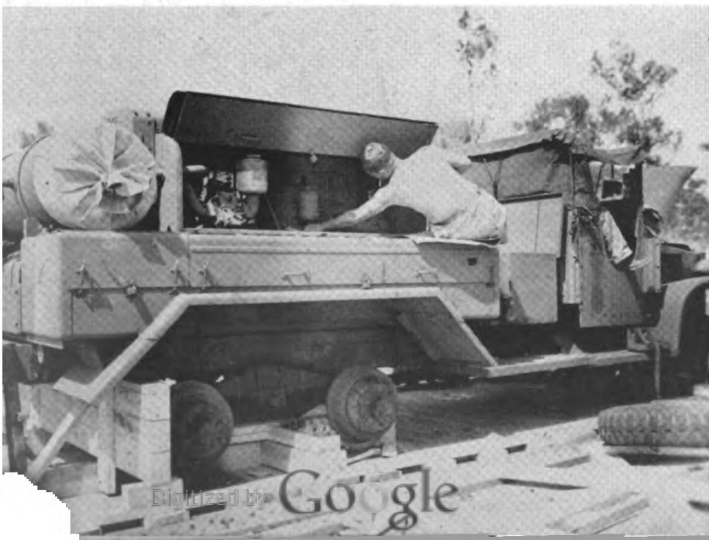


Kay, Harris, & Cox assemble trucks.

CRATED PO

Zito works on compressor.

3/4-ton fresh out of the crate.





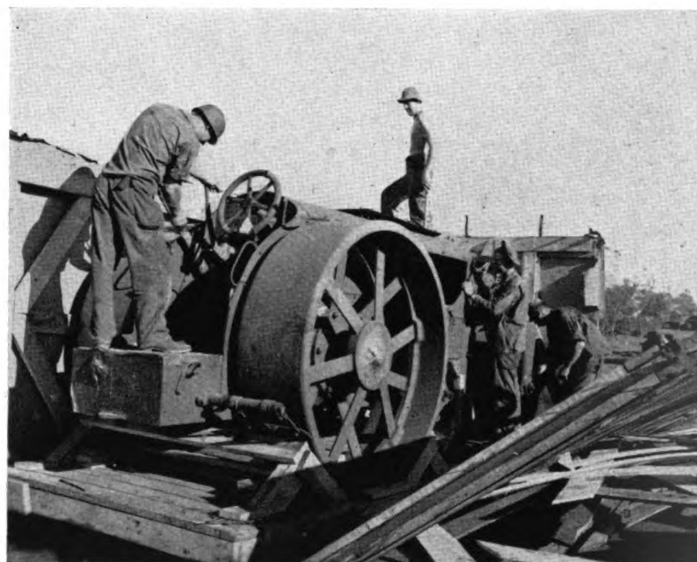
Wychor's pride and joy.



4-ton being freed.

MOTOR OL

Nickel, Brugman, Mooney,
& Roberts giving a flat roller
the business.



Sumski & Glogowski getting IR compressor ready for
duty.

8 yard carryall—before.





Darwin "Express" Derailed.



Sign painter Gambling at work.



Our secret weapon.

WORK BEGINS AT DARWIN

It was a very hot April day, the fourth of April to be exact, when the 340th Engineers sailed into Darwin, Australia. The heat was beyond anything the unit, as a whole, had experienced anywhere. The climate of northern Australia is naturally hot and dry from March to August; and supplying the population of Darwin with fresh water during these months presents quite a problem. Darwin wasn't a pleasant sight with its heat, dust and bombed ruins—the story behind the inhuman destruction is unbelievable. The unit was quartered for the night at the R.A.A.F. Field a short distance from Darwin. The unit was bivouaced at the airport for two days, during which time details were sent out to clear the new area, five miles from Darwin.

On April 6, 1944, the regiment moved to the new camp area where the men set up temporary shelters until pyramidal tents were furnished. Shower and laundry facilities were provided by the water purification element of the regiment at Rapid Creek, one quarter mile from the camp site. The motor pool immediately began to uncrate and assemble the regimental vehicles and equipment. A chapel and theater were constructed, and a P.X. and Regimental tents were set up.

The regiment began work on its assigned mission on April 7, 1944 involving construction of the Fourth Air Depot and allied projects for use by a B-29 group. The phases of this work were: Tarmac areas, quarry and crusher operation, sand and gravel pit operations, concrete work, clearing and grading of roads and building sites, erection of warehouses and hangars, construction of bomb dispersal areas, roads, and avgas distribution system.

During the month of May, work on the Fourth Air Depot continued—and the pace of the accomplishments of the regiment was kept at a high and uniform tempo, with the companies of the regiment utilizing equipment to the fullest extent by operating in shifts, twenty-four hours per day. Also, during this month, unloading details were furnished for ships arriving at Darwin with equipment or materials needed for the 340th and the construction of the Air Depot. As of May 31, 918 EM were engaged at Air Depot #4 or on projects contributing to its construction; 5 EM were engaged at the Engineer Warehouse; and 52 EM were working at Base One.

In June the principal assignment, Air Depot #4, remained unchanged. By the end of the month the work was 90 per cent completed, all but three phases of the project had been finished. Construction work brought to completion during the month included the following: rock crusher operations; avgas pipe line, with two 2,000 bbl. storage tanks; check station; telephone booth; pumps, pump station, and 34,090 feet

of pipeline tested for leaks and blocked up; 17 miles of road; two Butler hangars; sewer and water reticulation systems for the permanent camp area; 1000 feet of sewer line and 6,200 feet of water line; the digging, hauling and compaction of 29,626 cubic yards of gravel for the tarmac area and 11,169 cubic yards for the roads in the permanent camp and warehouse areas; application of 75 per cent of the primer coat of bitumen; production and hauling of 5,518 cubic yards of concrete.

One company was assigned the mission of erecting 16 portable steel Comet huts, 20'x60' with concrete floors, which were used by the U.S.A.A.F. for operational purposes. This project was a R.A.A.F. responsibility; but because of the inability of that organization to supply the necessary manpower, and at their request, the Commanding Officer, Base One, secured authority from the Commanding General, U.S.A.S.O.S. to use United States troops on this project and on any similar project of high priority.

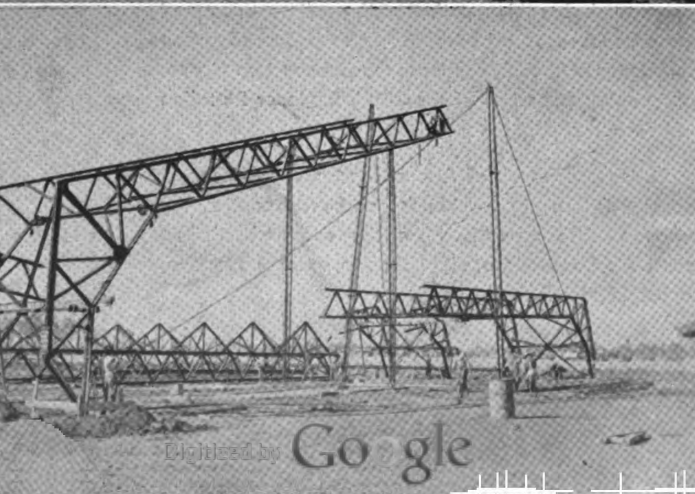
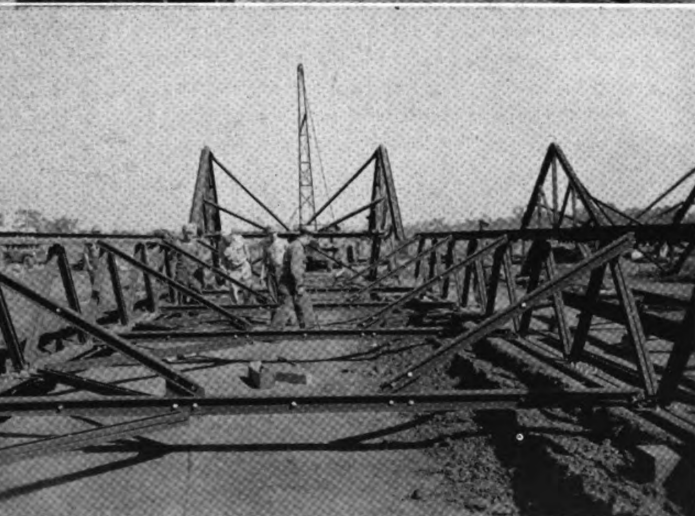
The regiment, on June 26, took over work for the completion of the 380th Bomb Group Encampment, comprising some 202 various types of buildings.

Also during the month of June, the regiment was assigned to an operational role in the event of enemy land operations against the Darwin area. Fox-holes were dug at the rear of each tent as a security measure in the event the enemy struck with air power. There were 2 air raids, caused by Jap reconnaissance planes.

Construction of the 4th Air Depot was brought to completion on July 25, 1944. Much of the work during the month was varied in nature and consisted in general of placing the finishing touches on phases of the project. The construction of the Air Depot was completed practically one month ahead of the time schedule. All phases of the construction were coordinated during that period in such a manner as to permit constantly increasing use of allied facilities by depot personnel. The unit received a commendation from Lt. Col. L. O. Cox, Project Engineer, for its work at Darwin.

It was during this month that the unit was changed from a regiment to a battalion. Colonel Corey became commander of the 1180th Engineer Construction Group, relinquishing command of the 340th Engineer Battalion to Major Maxwell.

During the month of August the unit was primarily engaged in the preparation for and movement of the battalion. The operations on the assigned mission, Fourth Air Depot, located at Darwin, ceased at noon, August 19, 1944.



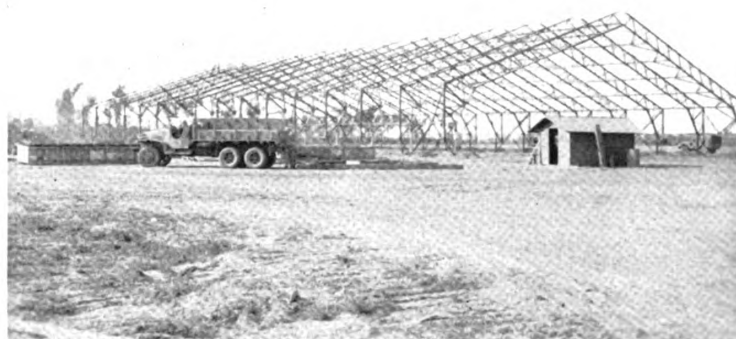
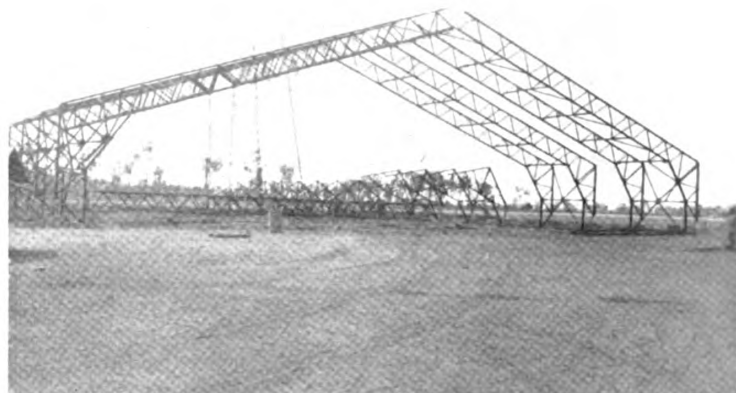
BUTLER

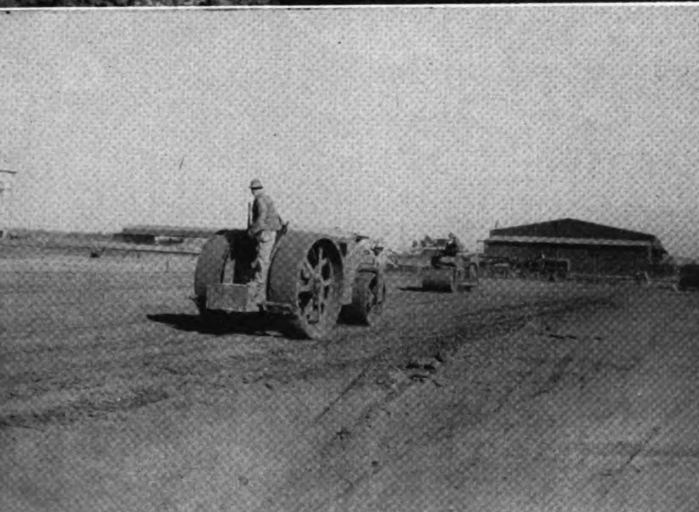
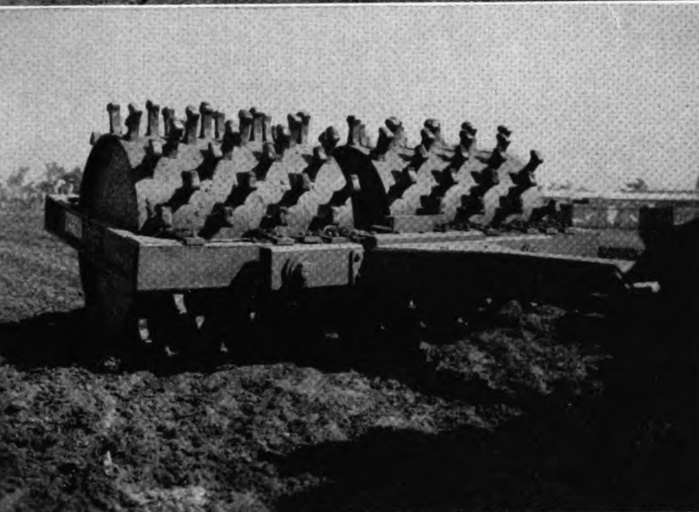
These hangars were constructed by Company E of the 340th Engineers. Each platoon was given the construction of a single hangar. Two modified Butler hangars were constructed at the RAAF field. These were modified to the extent that two 240 foot hangars were built from three standard 160 foot hangars. The only difficulty encountered in the modification was in the canvas roof since there were two extra end sections and two missing intermediate sections. The intermediate sections were made by bolting together two end sections with thin flat metal strips along the top and bottom of the lap. This wasn't, however, entirely satisfactory. The hinged trusses were usually raised in pairs using 4 gin poles. Later a crane and two gin poles were used, adding a great deal to the ever increasing efficiency of the unit. The width of these hangars or

HANGARS

distance between haunches was 180 feet. Base plates were set on concrete and held in place by anchor bolts rather than using drift pins to anchor the plates to the ground.

The sections were formed on an assembly line system away from the hangar site, to keep from interfering with the erection at the hangar site. All of the trusses were lifted into place by the use of crane and gin poles. One engineer platoon can erect one hangar satisfactorily, but the use of five squads will increase the speed of erection. A difficult problem concerning the completion of the hangar was raising the canvas. Canvas must be handled carefully to avoid tearing; rigging was a tough job. Company D poured the concrete floors.





TARMAC AREA

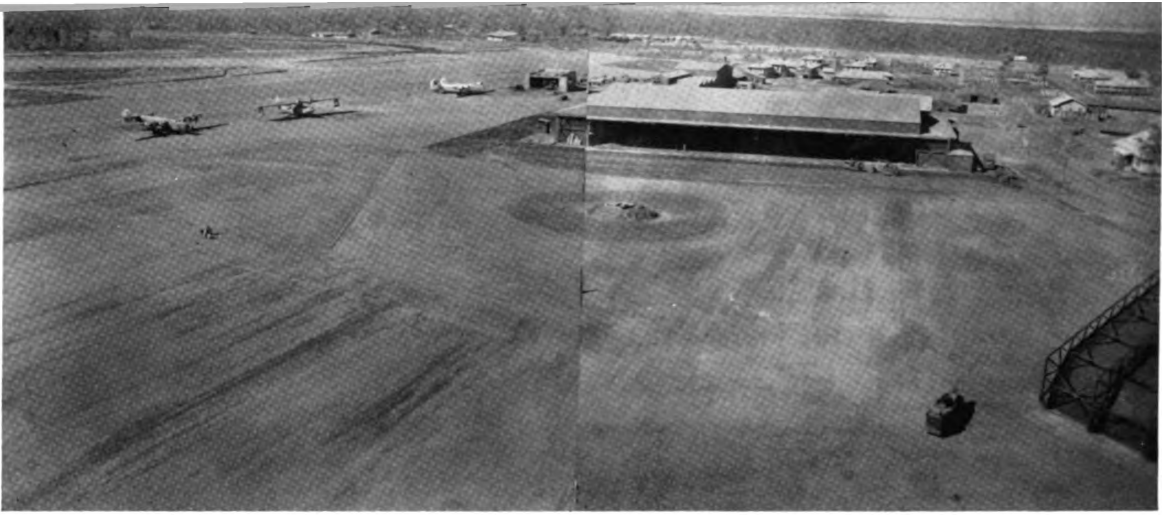
The Tarmac Area consisted of a single large hardstanding, roughly trapezoidal in shape. Dispersed throughout this surfaced area were five hangars, two reconditioned steel truss type, two new Butler type, and one large B-29 hangar of wooden arch construction. The area covered was 200,000 square yards and it required 70,000 cu. yds. of fill.

The hardstanding was intended for the use of B-29's and was designed to carry $\frac{2}{3}$ of a full B-29 load (approximately 70#/sq. in.) on the assumption that the planes would not be carrying a bomb load nor be completely gassed in this area. The bearing capacity of the sub-grade dictated a nine inch gravel fill to attain this capacity. The wearing surface was of asphalt construction with two seal coats of crushed rock.

The chief construction difficulties were in the location and compaction of old bomb craters, the location of suitable gravel deposits of a large quantity, and the moisture control during compaction of the fill.

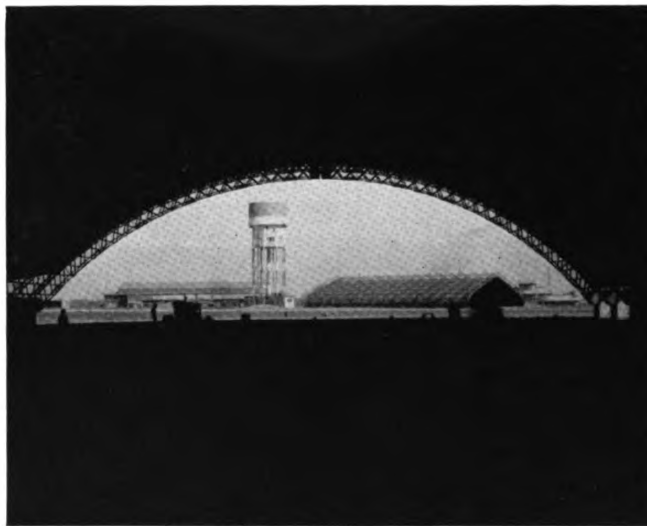
This project was especially interesting because of the high standard of earth work required.





LIBERATOR HANGAR

The main hangar shown here is one of the original RAAF facilities which was badly damaged by Japanese bombs. Several of the roof trusses were smashed and the roof itself was badly shattered. Another unit performed the work on the building itself and Co. D later poured a new concrete floor. In the foreground is the Tarmac area as it neared completion. The Catalina patrol boat stood by as a rescue craft in case any of the 380th Bomb Group Liberators went down at sea.



Inside looking out at "D" Company Butler Hangar and water tower.

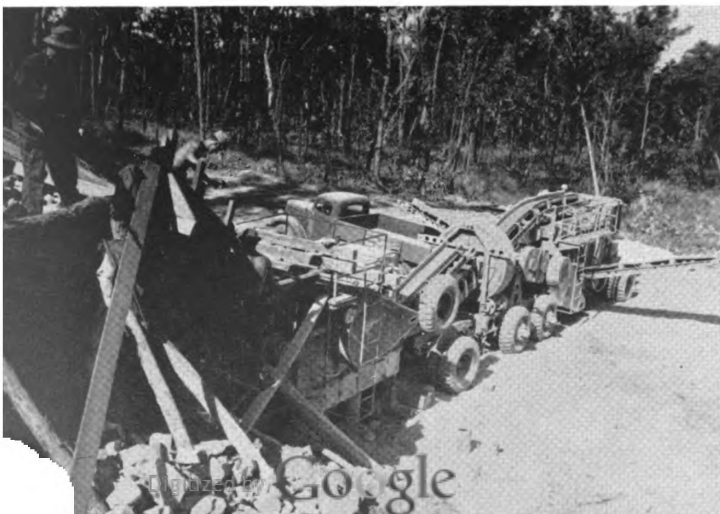
Wooden arch truss hangar fabricated and erected by A.W.C.





COMPANY "A" ROCK QUARRY

This crushing unit provided all the rock for the concrete work of the Regiment as well as the wearing course for the Tarmac area. "A" Company was a little perturbed at first because the location of the quarry was changed just after the first set-up was completed. The final location was much more conveniently located in a bed of "percillinite" at the edge of Darwin. Sgts. Moschetti and Romaine deserve great credit for their expert supervision of the blasting and crushing, which aided in the high rate of production—454 cubic yards of crushed rock averaged per 12 hour shift. Sgt. Graves' and Sgt. Parisi's two-yard shovel was easily able to keep ahead of the job. The success of this phase of our operation was assured by excellent organization.



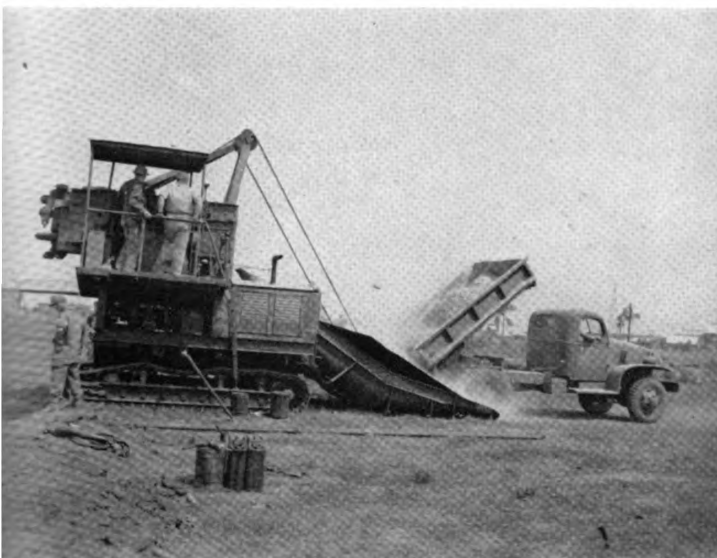


Unloading cement at the paver.



Clam Shell loads batching plant.

CONCRETE OPERATIONS



Paver gobbles aggregate.



And concrete comes out into Maloney's Chev.

Finisher smooths it out.





Barber-Greene Loader in operation.



Cat & carryall combination does its stuff.

Nolot on a D-4 does some clearing in the Air Corps camp area—



While Koob and Young on a grader do the fine work.



Well driller in action.



Erecting "F" Company kitchen.



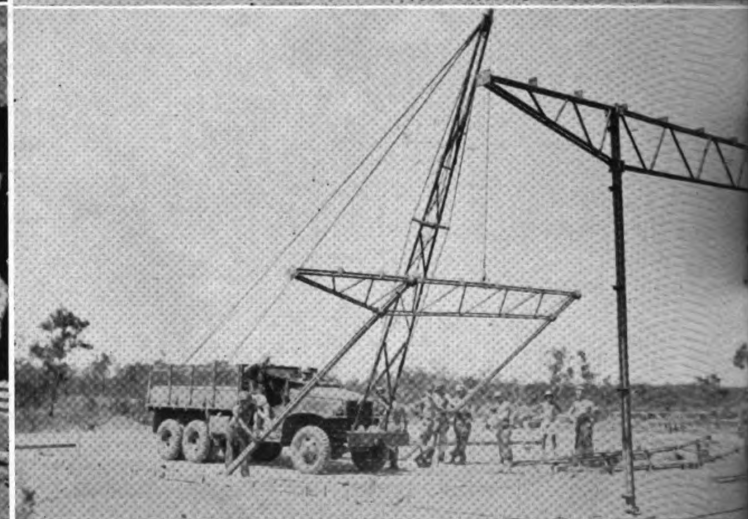
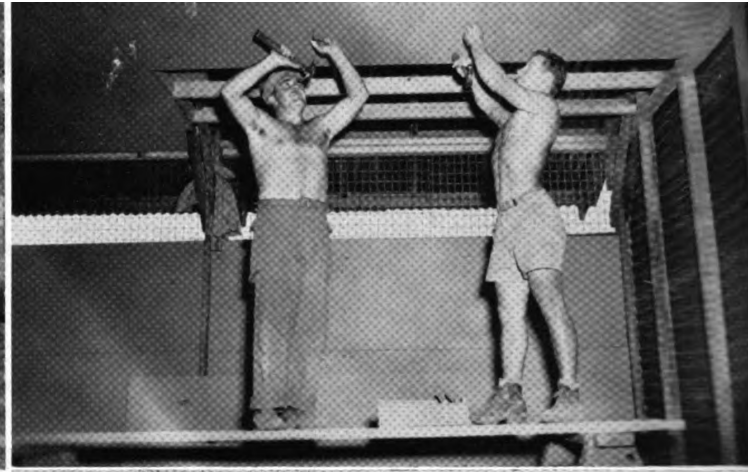
Sutkus assembling mess tables.

Post holes for truck-unloading platform—rush job.



Putting finishing touches on well pump.







WAREHOUSES AT DARWIN

Five cyclone type hangars were constructed to be used as warehouses. These buildings consisted of eleven tubular steel, prefabricated-type trusses. Each truss was made up of six columns and seven truss sections. The six columns extended to the roof and the intermediate truss sections bolted between the columns. The steel prefabricated footings for the columns were set in concrete. All the steel connections were bolted.

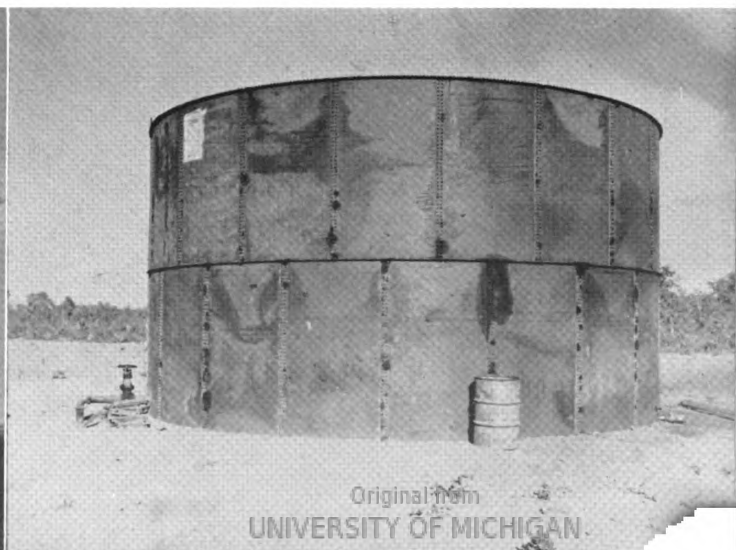
One platoon (30 men) erected the steel for a complete warehouse in seven to eight hours. In all the erections, none of the bolts were tightened until the complete truss was erected. A special boom was built and placed on the front of a 6 x 6-2½ ton GMC truck. All the truss sections were erected with this truck crane. The buildings were completed by bolting on

2 x 6 purlins and girts and sheathing with corrugated iron. The total time for the erection of a complete building averaged eight working days for a platoon of 30 men.

Company "E" erected five warehouses, two of these were for QM and three for AC. Company "E" also put in the foundations for loading platforms; while the Australians completed the platform and roof. "E" Company built 5 Ordnance stock buildings 54' x 108', during which time the Aussies built a few similar type warehouses. Company "F" was assigned to the job of preparing warehouse sites and in addition built two Signal Corps warehouses. The floors of all warehouses were of pre-mixed concrete and were poured by Company "D". The 340th was way ahead of the Aussies in building warehouses, among other things.



ASSEMBLING AVGAS TANKS.





BOMB DUMP AREA





Warehouse area and access road.



Taxiway and hardstanding.



Regimental camp area at edge of field.

RAAF AIRDROME

Overall views of airport.





THE LEPER

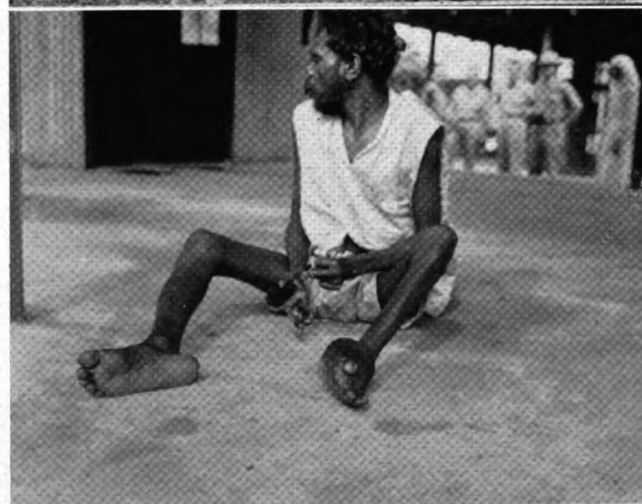
Across the bay from Darwin was Channel Island on which lived about 60 lepers, most of whom were Australian natives, "Abos". Three Belgian Catholic Sisters and an Aussie medical and administrative official gave injections of Chaulmoogra oil and otherwise cared for the lepers. The black patients living in pathetic C. I. shacks were a hap-



COLONY

py, chattering, care-free lot, even to the extent of free love.

Leprosy is essentially an infection of the nervous system, and so no pain is felt because of the disease. It is prevalent in tropical climates, but is contracted only after long intimate contact. Lepers usually die from secondary infections, pneumonia or other disease, not from leprosy itself.





MOROTAI HISTORY

On September 15, 1944 a convoy of one hundred ships sailed toward the southern tip of Morotai Island. This invasion point was only ten miles from Halmahera Island, where a force of 40,000 Japanese was stationed. The element of surprise was important.

After the softening-up process at 0800, H-hour, the 31st Infantry Division hit the beaches. A total of two Jap machine gun nests was put out of action.

The 340th cut several trails inland. Tons of supplies were unloaded into the water and onto the beaches.

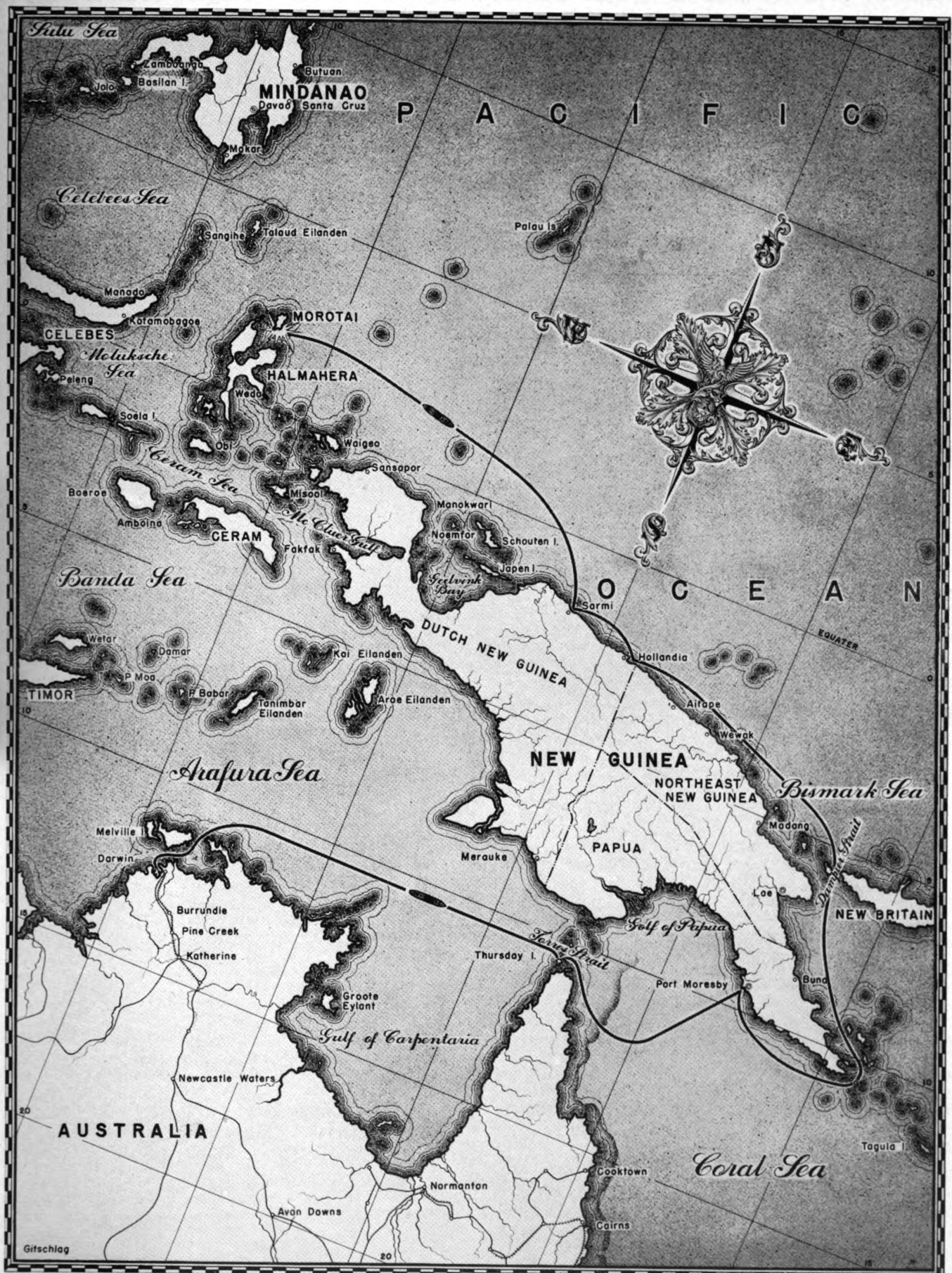
Before sixty days had passed, most of the docks, airfields and hospitals were completed. The initial primary mission of the battalion was to establish the main network of roads for traffic circulation, plus the construction of an avgas jetty. A major project was the S-shaped taxiway connecting the Wama Strip and Pitoe Airdrome. Other projects include a 500-bed station hospital; and the operation of a sawmill.

The 340th had its share of foxholes, and they were used frequently during the 102 days on the island. There were air-raid alerts almost every day for more than three months, as often as four and five times a night. On several occasions considerable damage was caused, especially to planes. The only bombs landing in the 340th camp area came December 13, 1944, and damaged two trucks, and put shrapnel holes in tents, cooking utensils and mess-kits.

The first Purple Heart of the battalion was awarded September 28, 1944.

Word came from the Commanding General of the 31st Infantry Division warning of a possible invasion by the enemy from Halmahera, on the night of 3-4 December 1944. The attack was to be by air, sea and ground. Extensive defense measures were set up by the battalion, barbed wire on the beaches, MG emplacements. However, the period of possible attack passed without action.

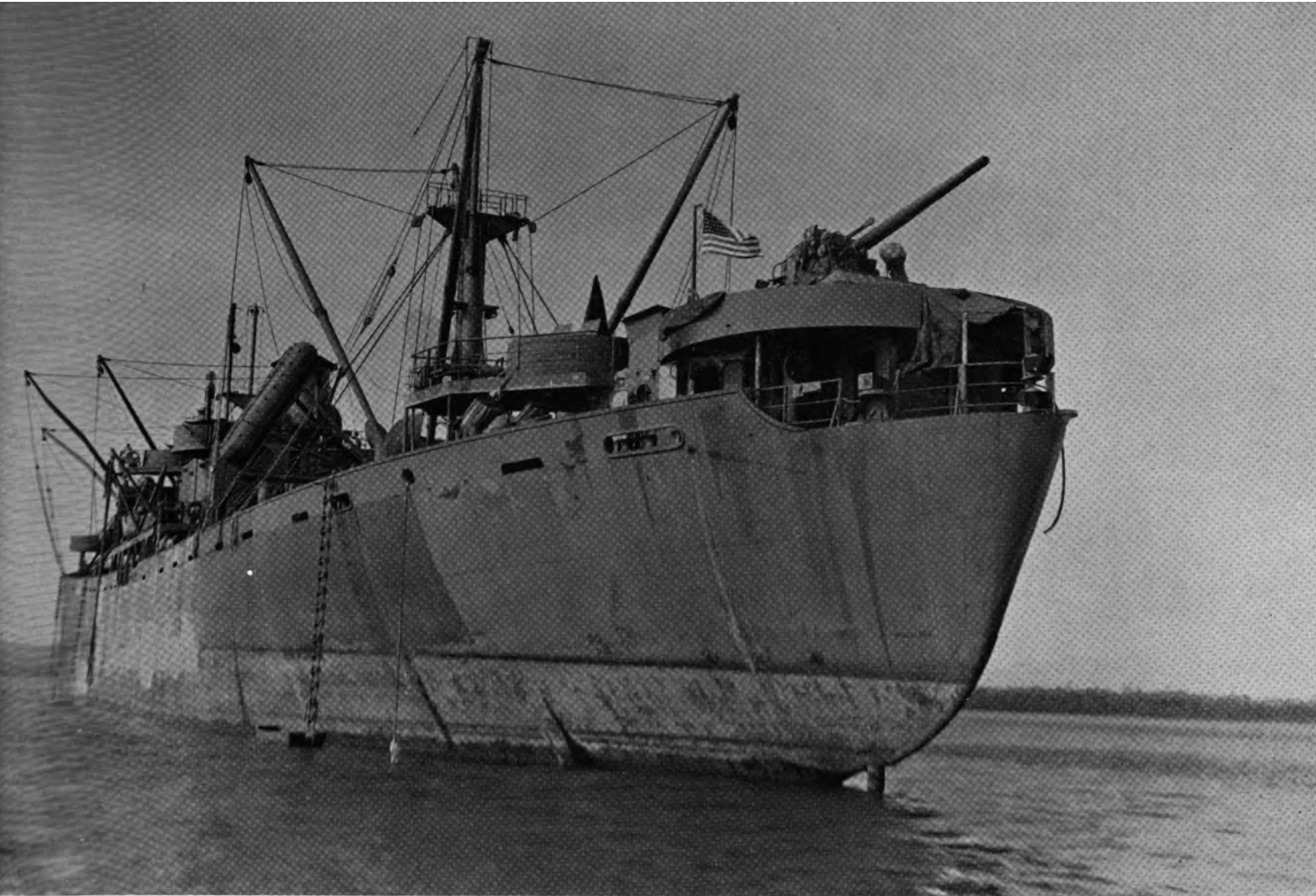
All construction operations ceased on December 20, 1944, to prepare for the coming M-1 operation. On December 26, the loading of equipment and personnel on two LSTs and one Liberty ship was completed. As usual, the enemy planes were over that night too. The remaining days in December and 9 days of January 1945, were spent enroute to the invasion of Lingayen Gulf, Luzon, in the Philippines.



Morotai

Moluccas Islands, East Indies





DON MARQUIS

On August 17, 1944, the entire battalion left Australia, enroute to Hollandia, Dutch New Guinea aboard two ships. One of these ships, the Don Marquis, is pictured on this page. It was by no means the most stylish way to travel;—the men slept in stuffy holds and ate chow on deck, under a broiling sun; nor could one say that the evening and early morning stand-to was appreciated by any of the members aboard, even though they were for the benefit and security of all. After sailing for thirteen days the men finally went ashore at Hollandia, August 29, 1944.

Yenor and "Speak to me, Joe."



Leo, Dies, and Lowe cleaning up.



Original from
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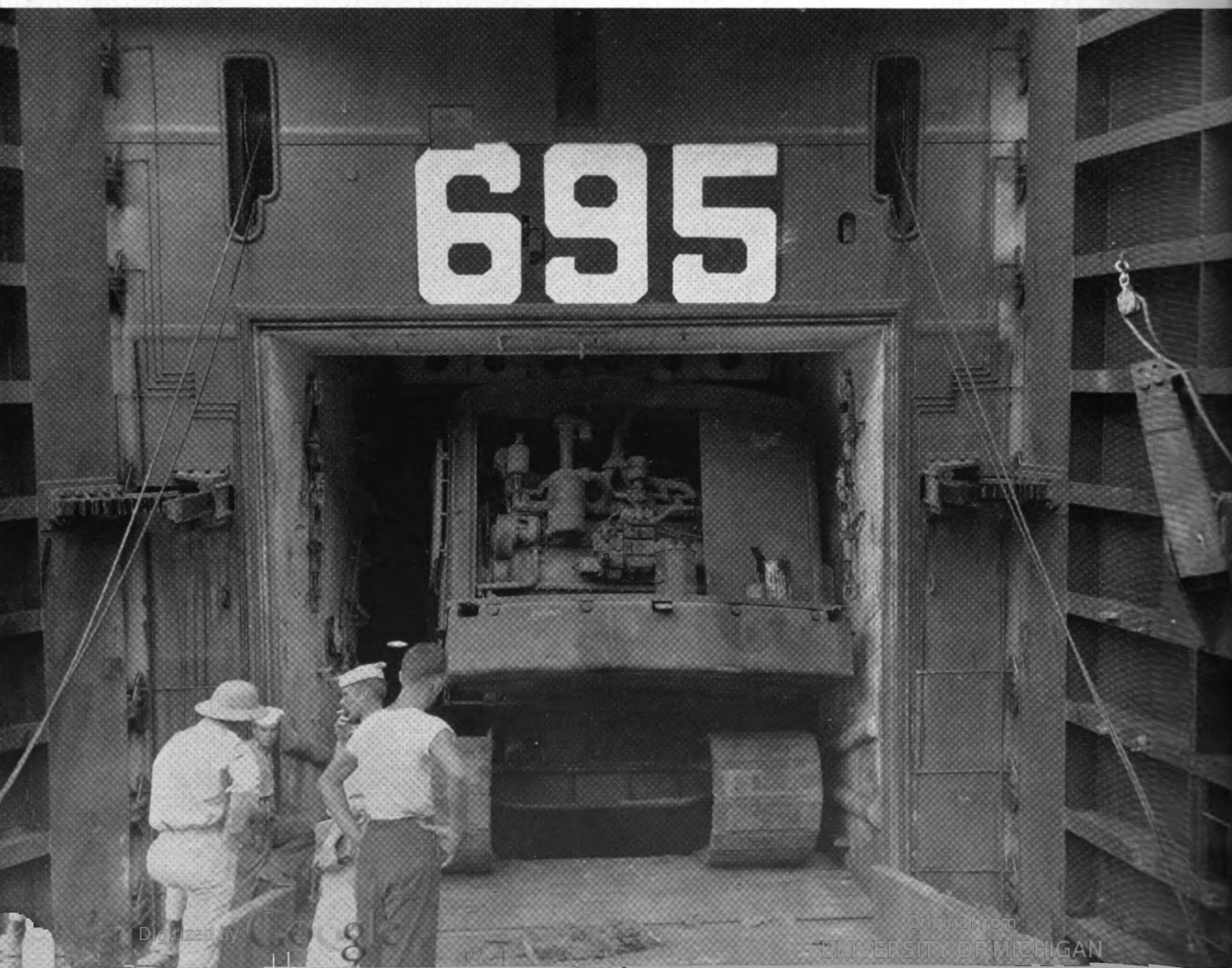


Building Loading Ramp at Hollandia.



Loading the 2yd shovel.

Anxious moments—it just fits.





Always room for one more.



General Quarters.

The Watch.



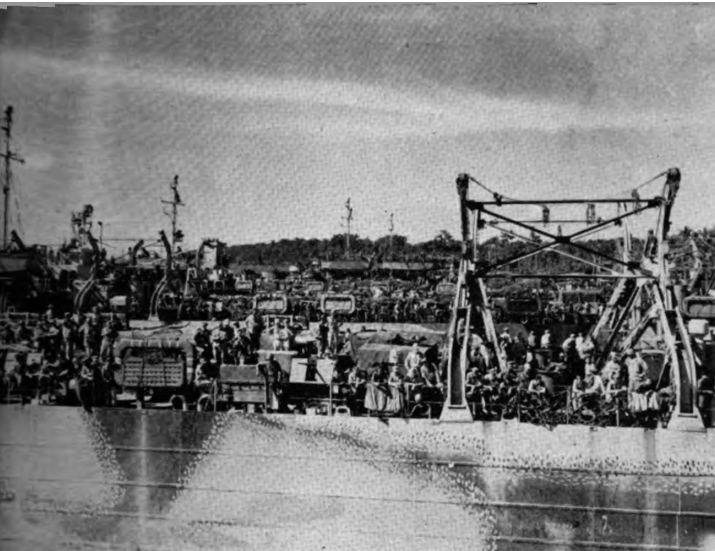
From Stem to Stern.





HITTING THE BEACH





Dress Right—What a Target!



Roll out the barrel.



Clear the Deck.

Crane bogged down in the sand —D-8 to the rescue.

Dragging out the Screener.





Tanks move up.



EARLY ROADWORK





A Company mess hall. D plus 15.

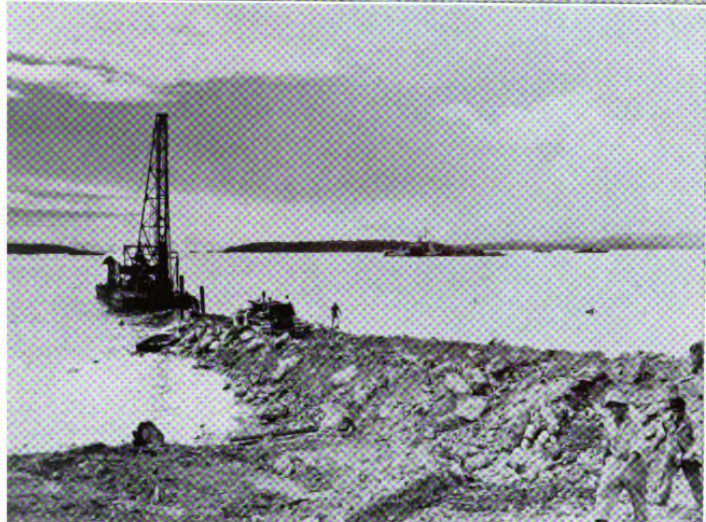


Wychor is first—as usual.

Headquarters on the beach at low tide.

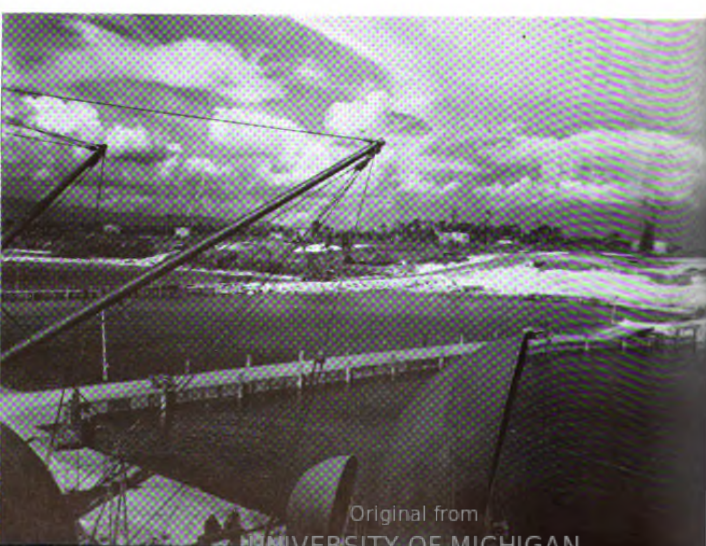
Battalion camp. D plus 30.





LIBERTY DOCKS

BEFORE
and
AFTER





LIBERTY DOCKS

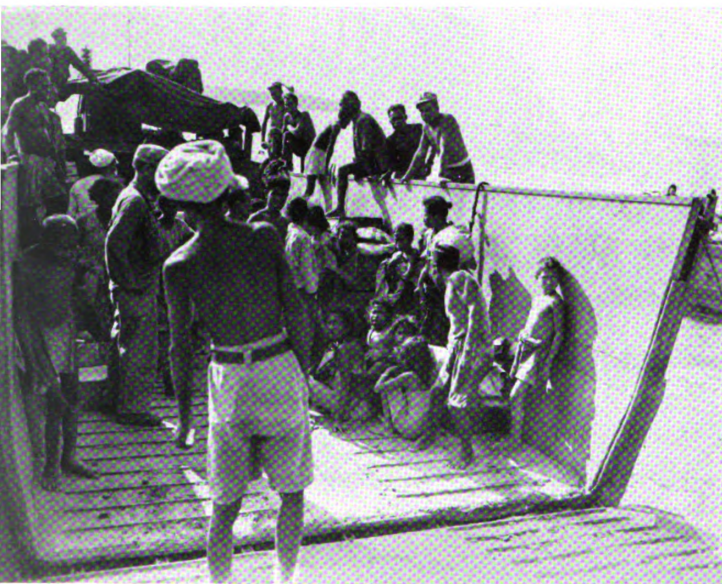
One of the first projects given the 340th upon arrival at Morotai was the construction of a jetty for carrying gasoline and oil pipelines. The work consisted of putting in a filled causeway one hundred feet into the sea, to which would be attached an extension of Navy ponton-cubes. The fill for the jetty was obtained from a nearby coral cliff by bold use of explosives, and pushed directly into the water with bulldozers. The ponton-cubes were anchored to piling driven from a barge, and the jetty was completed in short order.

The same scheme was used for the construction of a three-jetty Liberty ship dock. The docking facilities were provided by a double width of ponton-cubes connecting the ends of the jetties. The floating dock was long enough to accommodate two Liberty ships at a time. Company "C" carried out this construction assignment successfully.

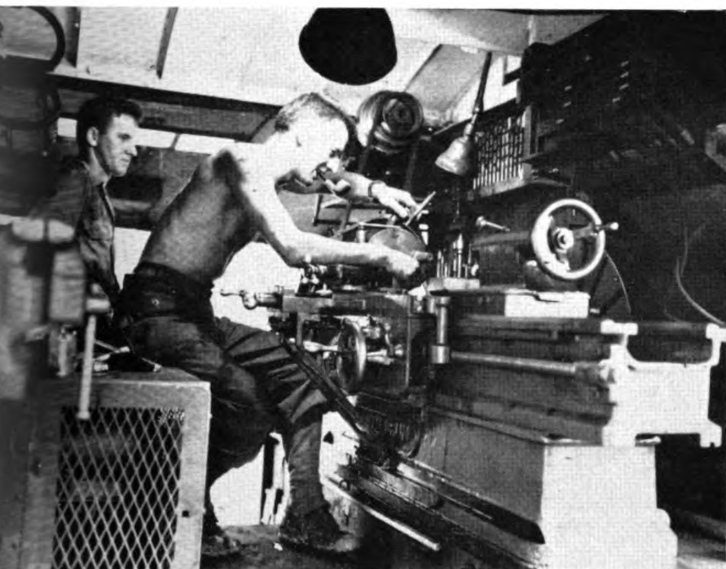


Burning the disease-infested village of Gotalalamo.

INDONESIANS ON MOROTAI



Amancio, Strickland, Triplett, Mellon, and H. Blake try to get parts.



Pesko and Wychor turning out parts.



Strickland at work.

Angotti takes over the job.



How to weld.





The Gulch Water Point operated by Vechio, Kirby, Brindamour & Floetke.

WATER D. P.

Water on Morotai presented quite a problem—a problem soon overcome by the 340th Engineers.

Thousands of troops were jeopardized because of the absence of fresh surface water; but after reconnaissance by unit operators, temporary water points were set up at two native wells, relieving the situation considerably; however, it was necessary to ration water for several days. Two more permanent wells were developed quickly to provide the needed water.

The 340th well-rig was put into operation to insure sufficient water. A total of 33 wells was drilled on the island. The average depth of these wells was 50 feet, ranging from 20 to 100 feet. The water level averaged 30 feet, the deepest being 90 feet. Water capacity, 10 to 100 gallons per minute. The 340th, by this time, was operating the main wells and, by D plus 4, was delivering over 200,000 gallons per day, using one portable and two mobile units. This pace continued for three months, fifteen hours a day.

Two tanks of water every hour.





Morotai Power & Light.



BSO.

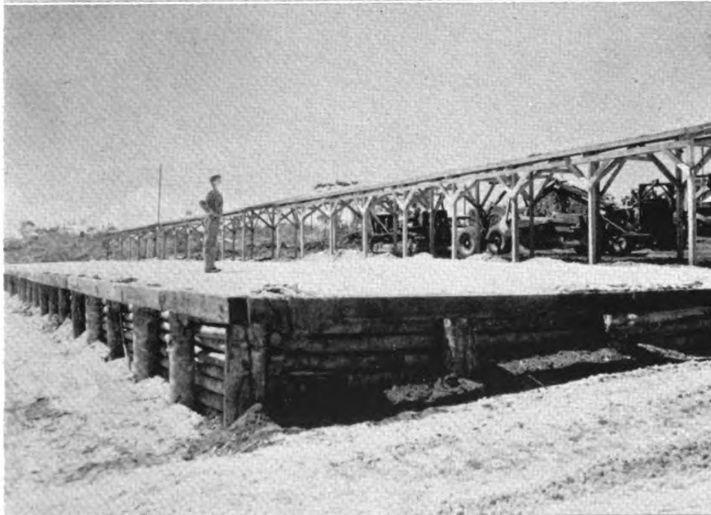
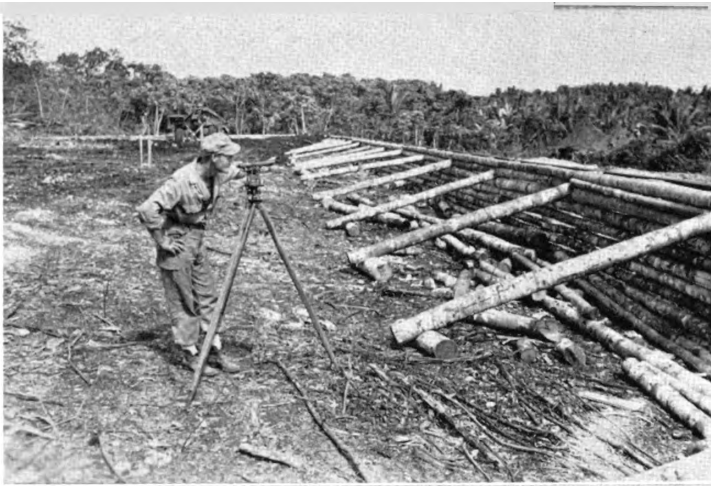


Morotai Board of Health & Dispensary.

Cyr's well rig.

Company A puts up Battalion Shower while Louden reads the Book of the Month, as usual.





DRUM FILLING PLANT

This particular job consisted of clearing a large area and erecting a loading platform on which gasoline and deisel oil barrels could be filled and then loaded on trucks. The platform was an earth-filled log crib. Allied work included building a shed over the platform, constructing storage sheds, erecting a pipe-stand to fill up individual tanker trucks, and constructing gravel roads around the area. Company "A" was responsible for the work.

Mention might be made of a gasoline spillway that was put up in record time by "A" Company. The spillway was an emergency project ordered by the 13th Airforce, when tankers scheduled to arrive at Morotai were overdue. Its purpose was to expedite transfer of airplane gasoline from drums to large storage tanks so that bombers and fighters could be kept fueled during the time of extensive raids on the Philippines. The job, including spillway, shed, and pumping unit, was begun at noon when the order was received; it was completed in a pouring rain at 1:00 a.m. the following morning. A personal commendation for the job done at a critical time was received by General Street of the 13th Airforce.

General Hall's Mansion — designed by Bacci.

Original from
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MORATAI SAWMILL

The sawmill on Morotai was operated by Company A, 340th Engineer Construction Battalion. The equipment used for this work was one Corinth Portable sawmill, two R-4 tractors, one $\frac{3}{4}$ yard motorized crane, and two 20-ton semi-trailers and one 16-ton flat bed trailer with prime mover. Twenty two men were required to operate the mill including checker and office clerk; fourteen men were used in the woods as tree cutting crews and one loading team. The average production of the mill was about 8000 FBM per eight hour shift, cutting mostly odd or small sizes.

It was learned that an extra half day spent in setting up the mill paid dividends in future production. The mill was set up some 2 to 3 feet above the ground to provide ready accessibility to the blowers for cleaning purposes and to give the conveyor height for loading trucks. The mill was equipped with twin gasoline engines which caused trouble after a certain amount of use due to the difficulty of keeping them synchronized.

Besides providing lumber for two station hospitals and various other miscellaneous construction at Morotai, this mill provided all the bridge timber taken by this unit as assault supplies for the invasion of Luzon. The heavy bridge timber produced came to 10,000 FBM per 8 hour day. The second platoon was in charge and operated this mill supplying lumber for bridges, hospitals and various other construction. Also, lumber for the Air Force and for trading material?





WAMA AND

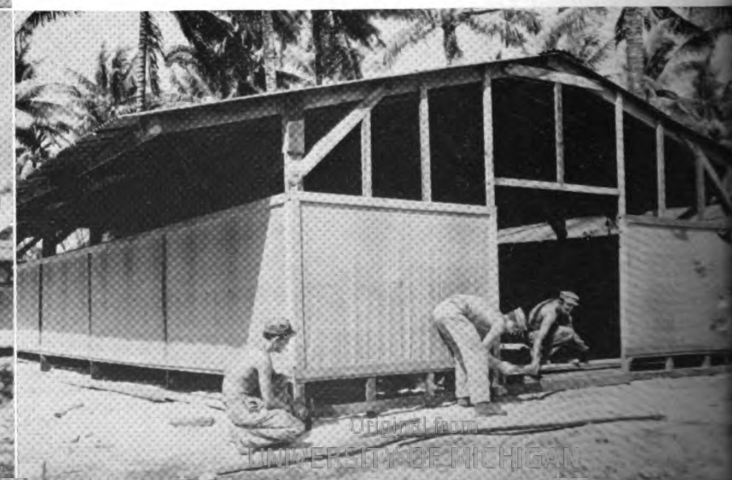
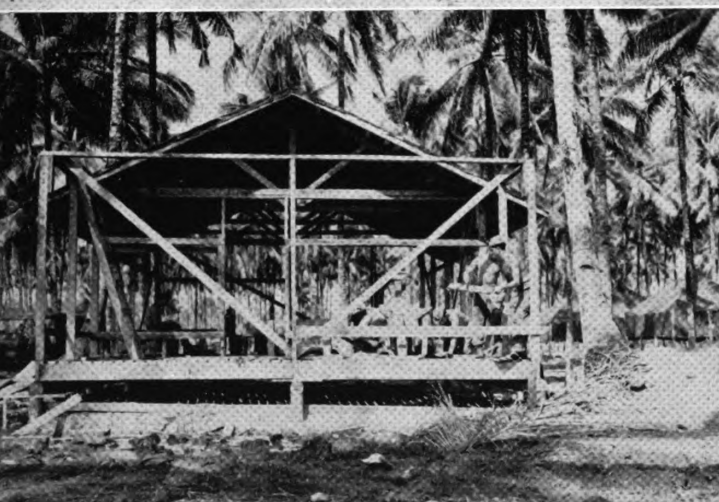
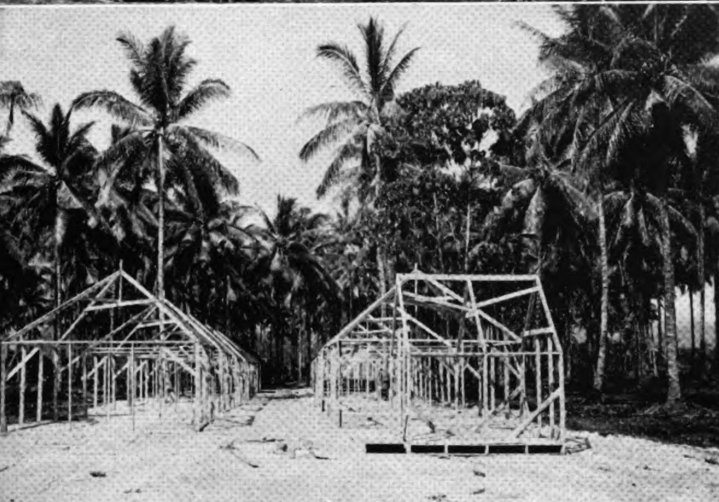
During the month of October, 1944, the battalion was engaged in the construction of base facilities on Morotai. A major project was the construction of a 2,942 foot "S" shaped taxiway connecting the Wama Strip with Pitoe Air-dome. It was necessary to



PITOE TAXIWAY

build this taxiway to enable some of the planes to take off. Both fields were long enough for all planes to land; but only the Wama Strip runway was long enough for the take-off of some of the faster planes. This taxiway was completed in eight days by Company "B".





MOROTAI HOSPITALS

The major accomplishment during November, 1944, was the completion of two station hospitals of 500 and 700 bed capacities. This construction included the installation of plumbing, lighting, sanitary facilities, quarters and recreation buildings.

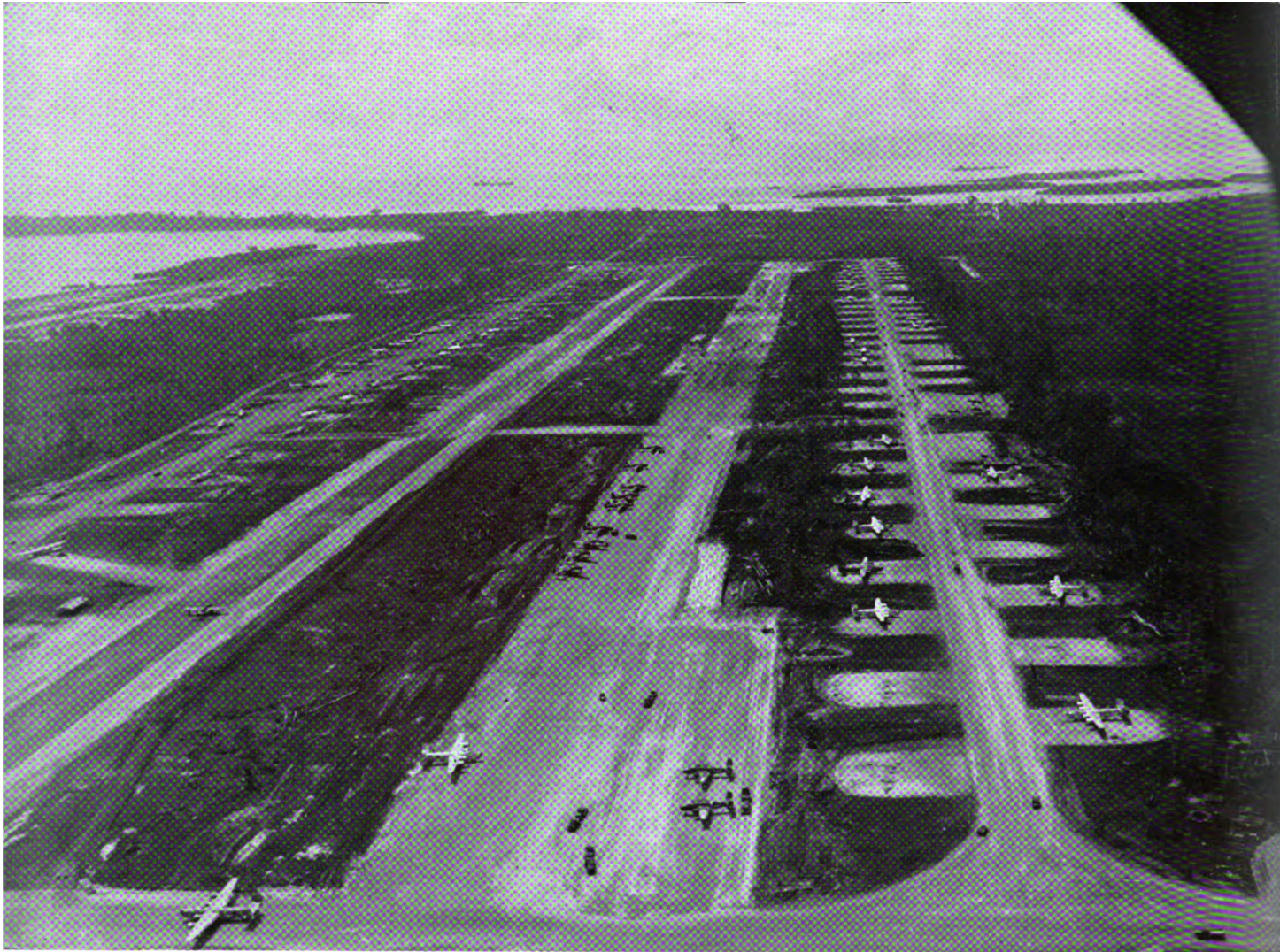
This job was scheduled for C Company, however, A, B and Headquarters Companies were in on it before it was completed. Company C handled most of the construction and set up the water system. Company B poured the concrete and helped with some of the construction. Headquarters was on hand to do the wiring and electrical work. During the construction of these hospitals three natives were killed when one of them hit a rifle grenade with his machete.



Aerial views of Tank Farm.

Late comers.





THE PITOE AND WAMA STRIPS. D PLUS 60





THUNDERBOLTS RETURN FROM MISSION



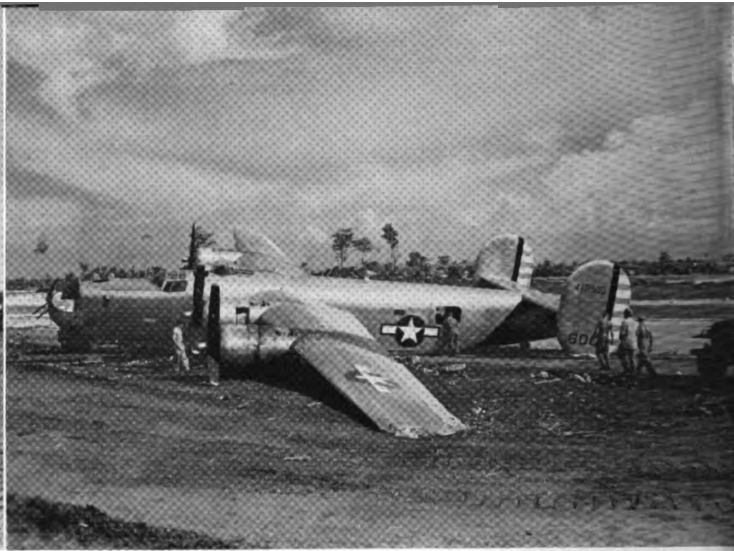
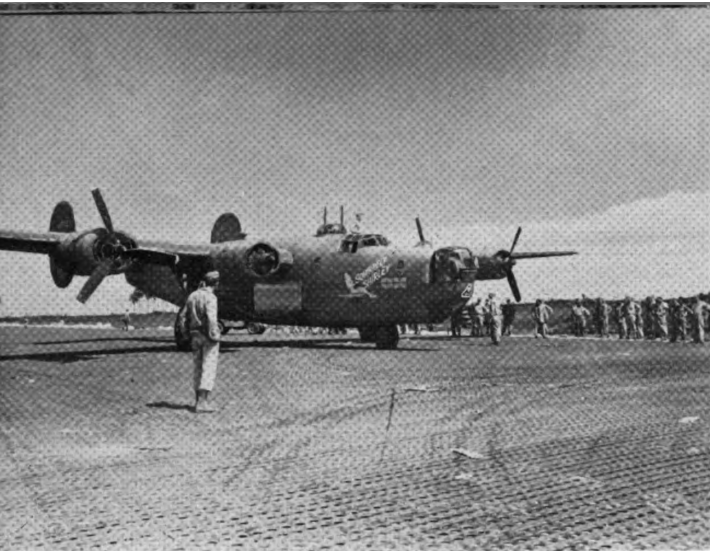


AIR CORPS





ART



LIBERATORS CRASH LAND ON RETURN
FROM FIRST BORNEO RAID





SUICIDE RAID

Not all Jap suicide missions were launched against ships as is evidenced by the destruction shown on this page. This pilot brought his two engine bomber to the Wama Strip in a perfect landing approach with landing lights on. However, instead of lining up on the runway he lined up on a long line of parked B-25's. The plane was not recognized as Nip until directly over the end of the field where one 40 mm gun and one multiple fifty took him under fire scoring several hits. The attacking plane continued on its course and crashed through several of the parked planes before disintegrating.

The wreckage was surrounded by men of the 340th before the pieces stopped falling and the loot included, one samurai sword, one Nambu pistol, Jap money, pictures and other knick-knacks.





Jap Zero reclaimed.

Nip Bomber





MISSION TO BORNEO

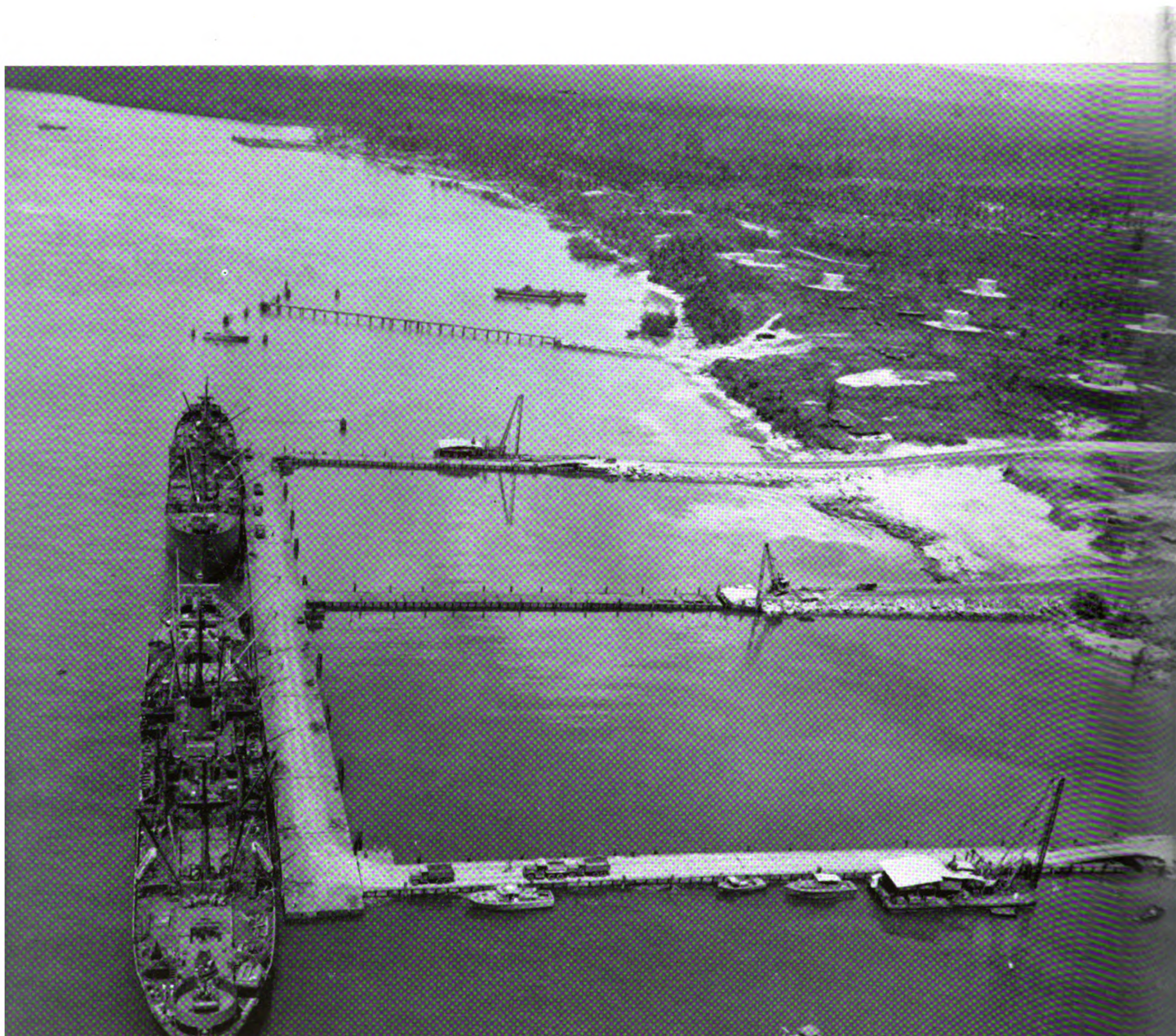
This is a report of a bombing mission as told by S/Sgt. H. K. Lee of 371st Bomber Squadron.

"We left Noemfoor at 2400, October 2, 1944. Our target was the large oil refineries at Balikpapan, Borneo. It took us 8 hours and 20 minutes to the refineries. 15 minutes before we reached our target, Zeros, Zekes, Hamps, Tonys, and Tojos came in on us like flies; the A-A fire was heavy and accurate.

"The Zeros made round-the-clock passes at us. Our lead ship 955 was the first to go down. Three of her crew bailed out. I didn't notice any of the others because there were too many planes around us. The nose gunner shot down two Zeros. The top turret got one; both waist gunners got one each. I got one: his plane blew up 100 feet under our ship. The tail gunner got two, and then the barrel of his gun burned out.

"Our plane was hit and oil came out of the number 2 engine, but we dropped our load and headed for home. The attack lasted 45 minutes, and twenty-four out of forty-two planes are believed to have been lost. We were losing so much fuel that we had to dispose of all unnecessary cargo. We headed for Morotai, and came in for the landing about 1700 3 October. When we hit the strip our right wheel locked and this sent us crashing into the trees along the strip. The whole ship seemed to crumble. However, Engineer Bowen was the only one hurt; his foot was pretty well banged up. The plane was a total wreck."

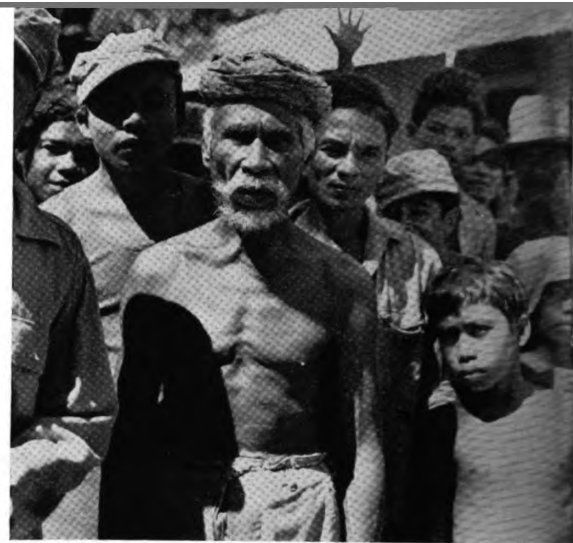




LIBERTY DOCKS AND OIL JETTY FROM AIR



MAIN STREET OF THE VILLAGE OF DEO



THE VILLAGE

The small Indonesian village of Deo is situated along the east coast of Morotai; approximately 40 miles north of where the 340th Engineers were camped and only a short distance from the northern tip of the island. The only possible approach to the village was by water; since the Japanese were in control of central Morotai, they were between the American forces and Deo.

Soon after the initial landing and after the southern peninsula was secured, American and Dutch authorities began rushing aid to this small village. Much needed food and medical supplies were brought in vast quantities. Everything of any material value was taken from these people by the Japanese. For three years these people had no medical supplies or contact with the outside world.

Traces of Japanese cruelty could be seen throughout the village. And every tropical malady imaginable was slowly taking life from these island people. Upon first sight these half-starved villagers feared the Americans, but it wasn't long before they became fast friends. This was something very strange





OF DEO

and new to them, for all they were able to remember was the ruthless Japanese. Both Dutch and American medical men gave aid to this lost colony.

A small school was reopened and the children seemed most eager to attend. Native instructors taught both English and Dutch. Older boys and girls attended classes to learn first aid which was being taught by an Indonesian doctor from Java.

Truly a scenic paradise, this little town of Deo. The main street is lined with giant cocoanut palms and walking under these palm trees one would see women washing clothes, men drying fish nets or working on their boats, children playing native games, older men chopping husks from cocoanuts and women drying fish to be served as a family treat. Down along the beach children play in the sand and not too far from shore older boys dive for pearls. Deo, whose population is almost 400 is slowly returning to its old way of life. Peace has now returned to this small scenic haven in the broad blue Pacific.





INDONESIAN MAN & BOY



LOADING AT MOROTAI --- CONVOY TO THE PHILIPPINES

All construction work on Morotai ceased on December 20, 1944. The equipment of the battalion was assembled in the motor pool for repairs and painting in preparation for the coming M-1 operation. The next stop was Luzon. At first there were only one LST and a small Liberty type cargo ship to load. However, another LST was sent in to handle the overload. On December 26, the loading of the LSTs and the cargo ship was completed. The loading continued into the night, and just to make it more difficult, during the night of the 26th, the island of Morotai was visited seven times by enemy aircraft. It was quite a send-off, with plenty of fireworks.

On December 27, 1944, the unit pulled out aboard LST 680, LST 126, and Liberty Ship A. K. Mercury. The decks were loaded with trucks, tractors, trailers, etc., all jammed into every available space, leaving little room for the men. They had to park day and night topside with a life-jacket for a pillow and the steel deck for a bed. Time was spent playing cards reading, talking about the invasion, crawling under the trucks during rainstorms, eating, sleeping.

The first leg of the voyage took the unit to Sansapor to join the bulk of the convoy. One evening while anchored at Sansapor, a lone enemy plane flew in a wide circle over the convoy. The search-lights found him; the anti-aircraft guns from land and sea opened up, and it was only a matter of minutes before the plane exploded and dropped into the bay.

The three ships having joined the large Naval Task Force off Sansapor, the convoy proceeded to its destination via Leyte, Surigao Strait, and the China Sea. This route took the convoy south of Negros, southwest of Panay, south of Mindoro, up the west coast of Mindoro, west of Manila Bay and Luzon, down into the Lingayen Gulf, and finally onto White and Blue beaches. The convoy was attacked on several occasions by enemy planes twice just off Mindoro. One suicide Jap plane crashed into an LST just ahead of the ships that carried the 340th. But the unit arrived safely on the morning of S-Day, January 9, 1945.





Luzon

Philippine Islands



LUZON

At dawn on the morning of "S-Day", January 9, 1945, the convoy entered Lingayen Gulf, Luzon, Philippine Islands. Heavy Naval shelling and dive bombing preceded the landing of troops. The first element of the Battalion beached at 1200 hours, "S-Day", followed shortly by the remainder of men and equipment. Enemy artillery and mortar fire were encountered during the landing on White Beach No. 2.

The Japanese had two 12-inch coastal guns well concealed in the hills near Rosario. These guns gave the 340th sleepless nights on the beach at Luzon. The unit was camped beside two U.S. Army batteries of 155's,—which were fighting it out with these enemy guns. After dark the men were able to see the flash of the enemy guns, and 27 seconds from the time of the flash, a thousand pound projectile would whistle overhead and burst somewhere beyond. Fortunately, the enemy never did find the range. The guns were finally silenced by the 158th Combat Team, the fighting Infantry, as they inched their way over the rugged hills toward Baguio.

Immediately after landing, the construction of a one-way, 30 ton capacity, pile bent bridge 104 feet long was started. This high-priority bridge was completed in 56 hours; work then proceeded to another bridge just beyond the first. Work on both of these bridges was carried out under blackout conditions. A by-pass road to carry the traffic was constructed by clearing the rails and ties from two miles of railroad right of way, and by placing treadway on the existing railway bridges. This was a very critical period of the invasion and roads and bridges were extremely important.

One particularly important operation was the simultaneous erection on the MSR of three Bailey Bridges: one 100 foot triple-single, one 70 foot double-single, and one 40 foot single-single. The work on these bridges was done at night to minimize interference with traffic. This work was temporarily interrupted by a Japanese infiltration and attack, but the bridges were completed in the prescribed time.

During the month of January 1945, the 340th Battalion was well dispersed. Company C was camped one mile east of San Fabian and later at Bani on Route 251. Headquarters moved from the Beach to vicinity of San Jacinto. Company A bivouaced at Binalonan on Route 3G, and then at Santa Maria. Bridges were built by A and B Companies along with maintenance of fords and by-passes. Company C was engaged east of Damortis in probing for mines, collecting and destroying Japanese ammunition, and blasting caves believed to be occupied by Japanese.

During the month of February the Battalion was engaged on various assigned missions, including road maintenance, bridge construction, construction of a cub strip, and sawmill operations. Approximately 54 miles of road were maintained, and 16,000 yards of gravel spread on road network. Company A took over the sawmill at Bitulok on the east coast February 18, 1945. Company B constructed a 100 by 1100 foot cub airplane strip near Manaoag. The Purple Heart was awarded to three members of the unit during this month.

The 340th was sent to work with the 25th Division on January 14, 1945. Early in the month Company C worked with the 158th Combat Team, pushing toward Rosario from Damortis.

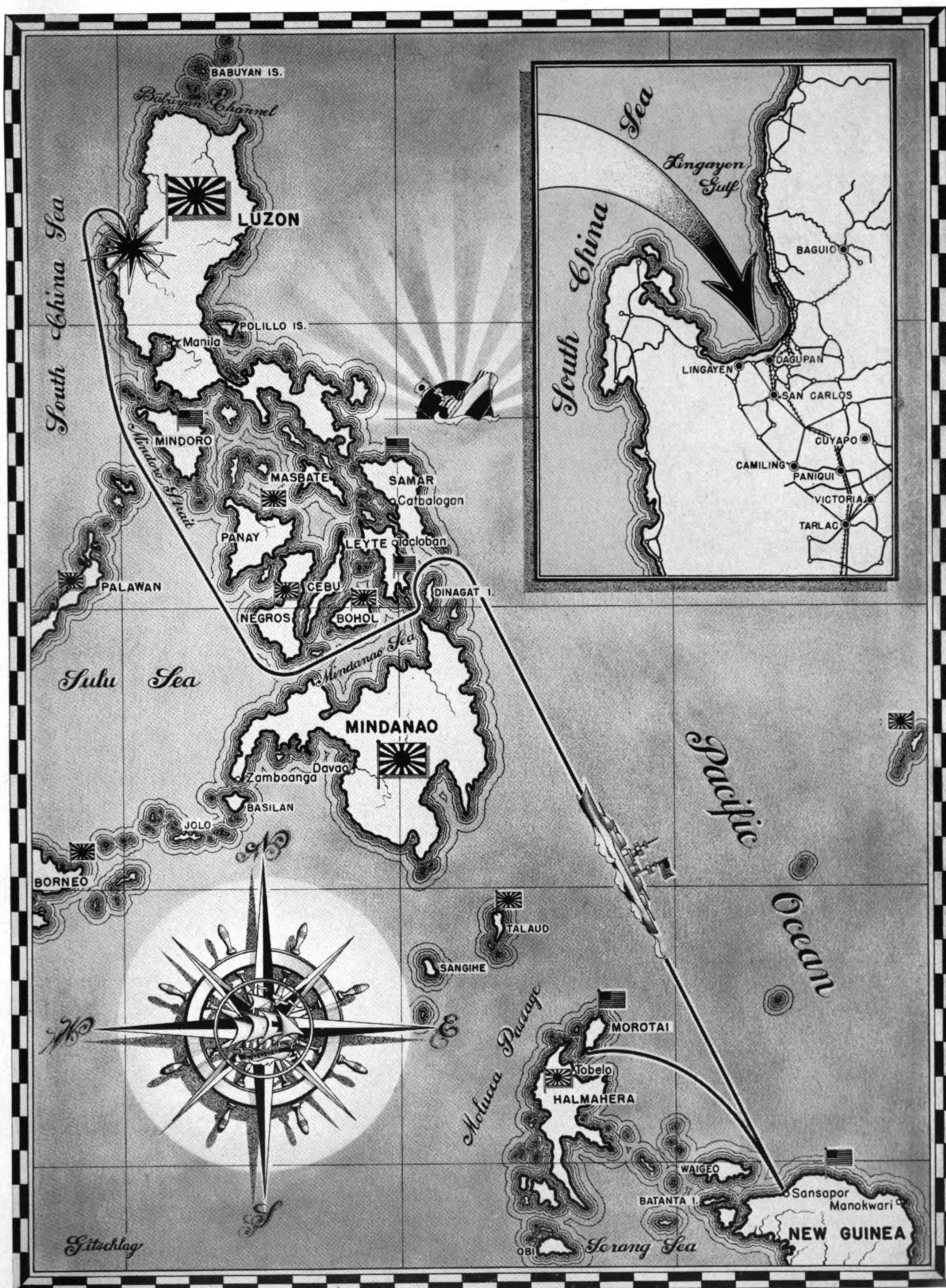
Headquarters, Headquarters and Service Company were stationed at the following places during the Luzon campaign: January 9, at White Beach No. 2; January 14, vicinity San Jacinto; latter part of January at Asingan; February 1, at San Pedro, near Rosales; April 24, north of Agoo; June 19, moved from Agoo to Santiago, Isabela Province; June 27, located at Tumauni; July 21, moved from Cagayan Valley to Camp John Hay at Baguio. And on October 15, moved from Baguio to the beach at Caba, during the latter days of the unit's existence.

Company A moved from San Fabian to 1 mile south of Binalonan on Route 3G, January 21, 1945, then on to Santa Maria at the end of January. Most of the work consisted of road maintenance, with time out to look for Japs infiltrated nearby. Their next move was 75 miles east. The company arrived on the east coast February 18 to take over the Bitulok sawmill. March 29, one officer and 50 EM left Bitulok for Pozorrubio. The remaining 150 EM and two officers set up camp north of Damortis. While at this camp Company A resurfaced 14 miles of road between Damortis and Aringay. Upon completion of the road Company A was sent up with the 37th Division to help maintain main supply route in Cagayan Valley. They pushed on up the Cagayan along with the rest of the unit working with the 25th, 32nd, 37th and 6th Divisions. July 22, Company A moved from Cagayan Valley to work on the road north of Baguio, joining Company C at K.P.-90 (Kilometer Post 90 N. of Baguio). Returned to Baguio late in September to help build a stockade for the surrendering Japs. From there to Caba, Manila and Home.

Company B was in the following places during the Luzon campaign: January 14, 1945, at San Jacinto; January 19, two miles south of Manaoag where they built a landing strip for L-5 planes. January 23, moved to vicinity of Urdaneta; February 20, one officer and 43 enlisted men moved from Urdaneta to Rosales; February 28, three officers and 167 EM moved from Urdaneta to Pozorrubio; March 11, the company moved from Pozorrubio to Mitura River; April 13, three officers moved along with 140 EM to Rosario; May 12, Company B moved to Baguio, Mountain Province, to join A and C Companies later; June 20, the company moved from Baguio to Ilagan, Cagayan Valley; July 21, Company B left Tuguegarao and moved to Route 11, near Camp 30, Mountain Province. Then on to Caba.

Company C moved from White Beach to one mile east of San Fabian on January 12, completing several bridges. January 21, the company moved to Bani on Route 251 to work with the 158th Combat Team; February 1, moved from Bani to Bolo, then on to Tayug; February 26, moved to Santa Maria; April 1, one officer and 50 enlisted men of Company C moved from Tayug to Aringay. By April 8, 1945, the entire company was together at Aringay. Company C then moved to Caba to build a station hospital. April 17, the company moved to within 3 miles east of Naguilian. Here the men were engaged in the construction of two long Bailey bridges within five miles of each other, opening the road for the 37th Division for their drive on Baguio. July 6, Company C moved from Caba to Suyu, and from Suyu to Baguio in September. Then down to Caba, early in October.

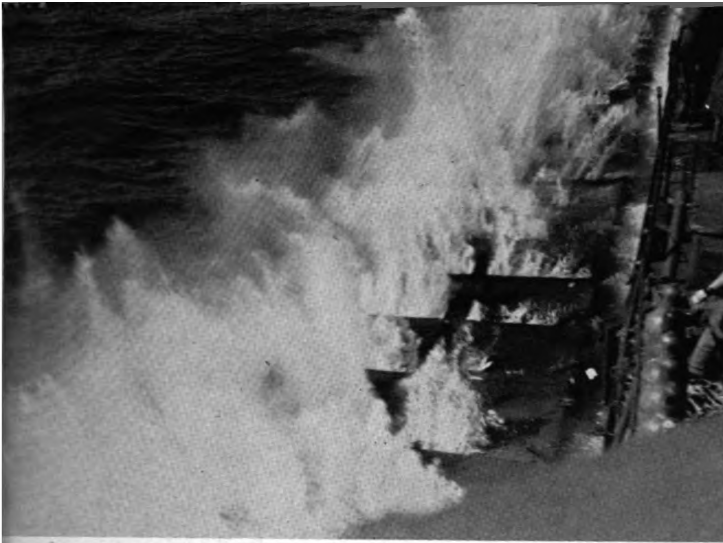
While at Caba the Battalion was in its last days, when all equipment and supplies were turned in. By October 29, 1945, most of the high point men were on their way home.





HEADING FOR THE BEACH





Dropping floating docks.



Late afternoon—S Day.

Just waiting.

Troops land at White Beach No. 2.





ARTILLERY DUEL

On January 9, 1945, the landing at Lingayen Gulf was the outfit's second invasion experience. They were old hands at taking enemy air attacks; but cannon and mortar fire were something new. For ten days the enemy threw cannon and mortar shells at White Beach and, incidentally, the 340th in the vicinity of San Fabian.

The enemy's 12 inch coastal guns were hidden in the hills near Rosario, probably 10 miles distant. U.S. Artillerymen were set up first to the rear of the 340th camp site. These Japanese guns were well dug-in behind a hill, and were very cleverly concealed under moveable Filipino huts which were rolled back and forth on steel rails.

Cruisers and destroyers fired on these hidden guns, and planes divebombed the entire area. The second day two batteries of 155's were set up less than a hundred yards behind the 340th Engineers. The cannon duel was on. In this case, the artilleryman couldn't see but knew what he was shooting at, because the enemy was shooting back. And the 340th was in the middle. This firing went on all night long. It was a glad day when the 158th silenced the Big Berthas.

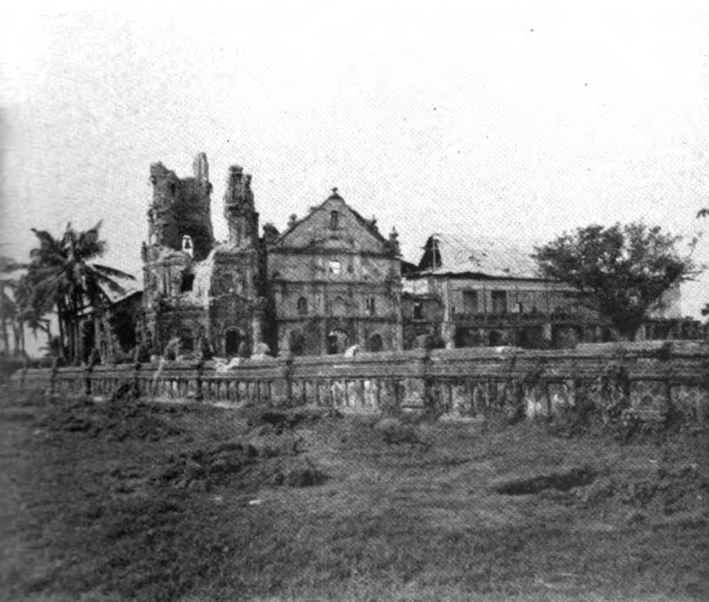




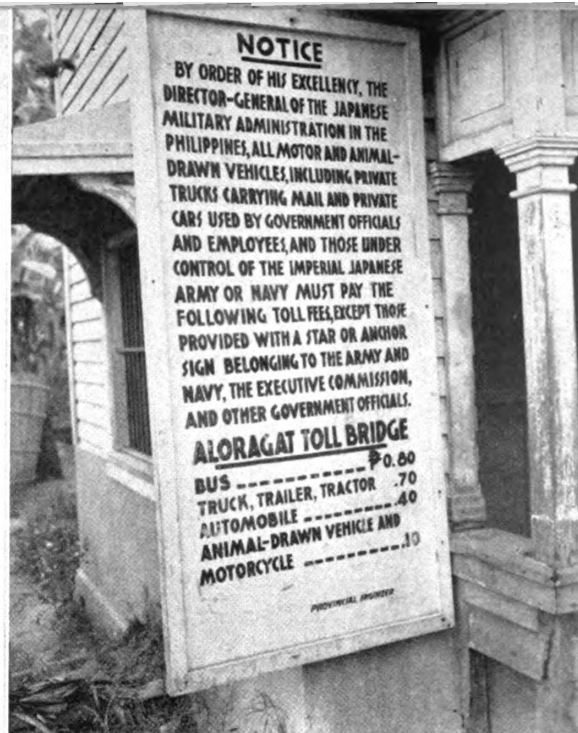
First bivouac on Luzon.



Company "A" builds first bridge on San Fabian road.



Digitized by Google
Spanish Mission in San Fabian.



Not applicable after our arrival.



Original from
33rd Division moves forward toward Rosario on Hwy 3.



THE TANK BATTLE OF SAN MANUEL

One of the sites of a famous Japanese "last ditch" stand was San Manuel. The 25th Infantry Division with its supporting heavy tanks proved too powerful for the enemy's well-concealed armor and entrenchments as the Nips again went down in defeat. A total of 45 tanks, several munition dumps, and countless enemy personnel were destroyed during this three day historic battle.



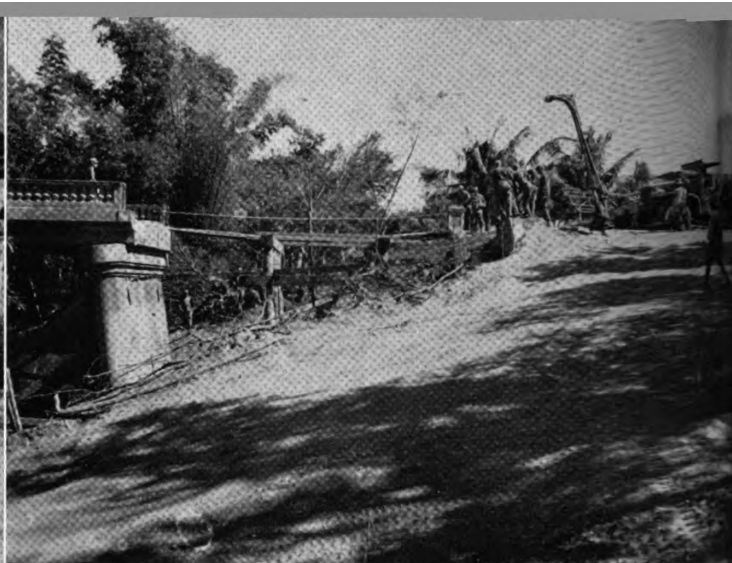


After the night attack on our first Bailey Bridges near San Fabian.



Victorious Filipino Guerillas . . .



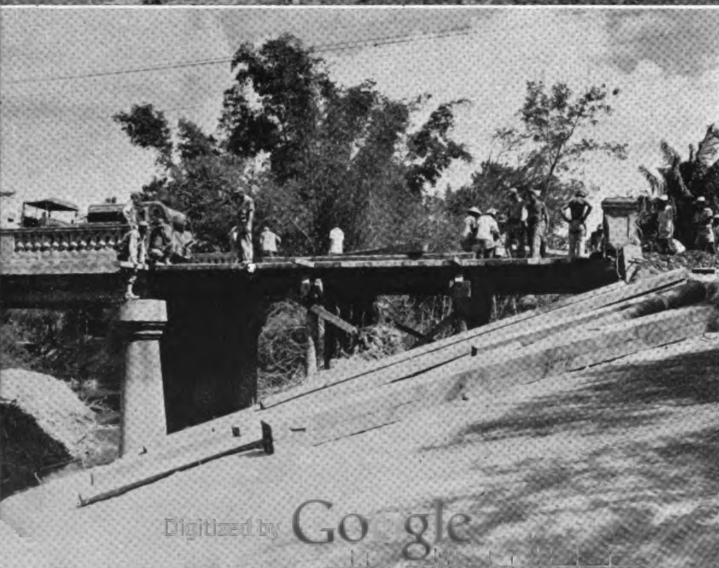


TAGUMISING RIVER BRIDGE

This bridge, on the modern concrete highway between Urdaneta and Binalonan, was originally a 4 span concrete bridge.

The most interesting phase of the operation was the restoration of one concrete pier to a vertical position.

By the use of a series of eye bolts fastened to cables, the pier was winched to a vertical position by using truck and tractor winches, after jacking up the adjacent span weighing 40 tons. Trestle bents were used in destroyed span.



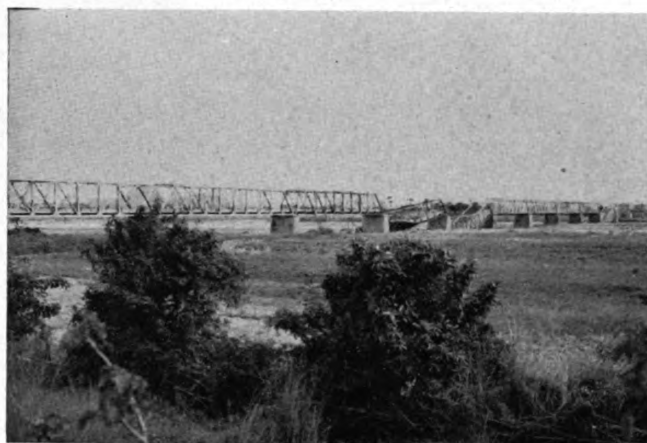


Alligators along White Beach.



Church at Tayug was destroyed by bombs.

Carmen Bridge across Agno River. 6th Division was fighting for possession of this bridge when picture was taken.

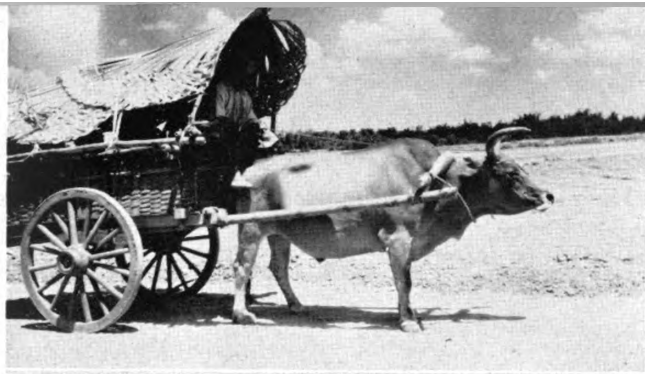


Captured Japanese motor pool at Rosales.



Destruction in San Fernando Bay.





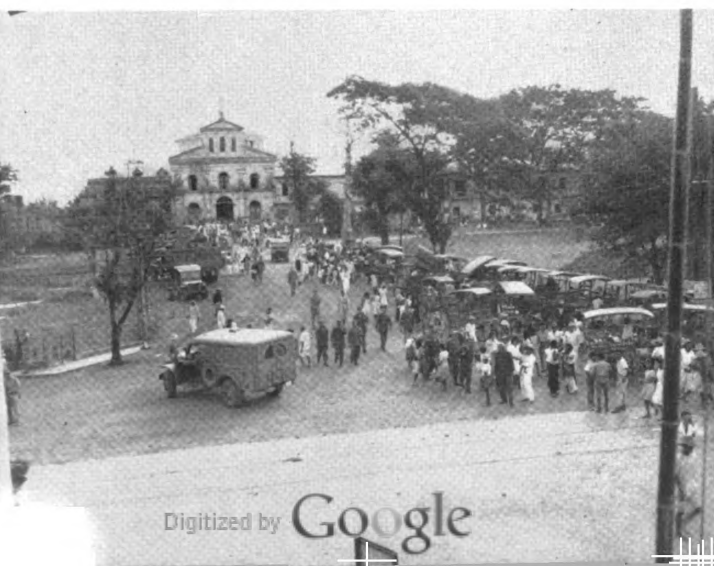
Filipino trucks and taxis.

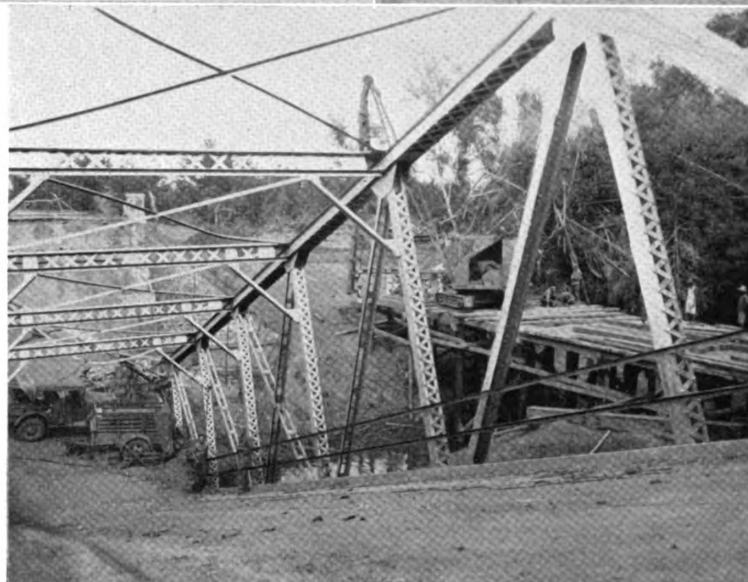
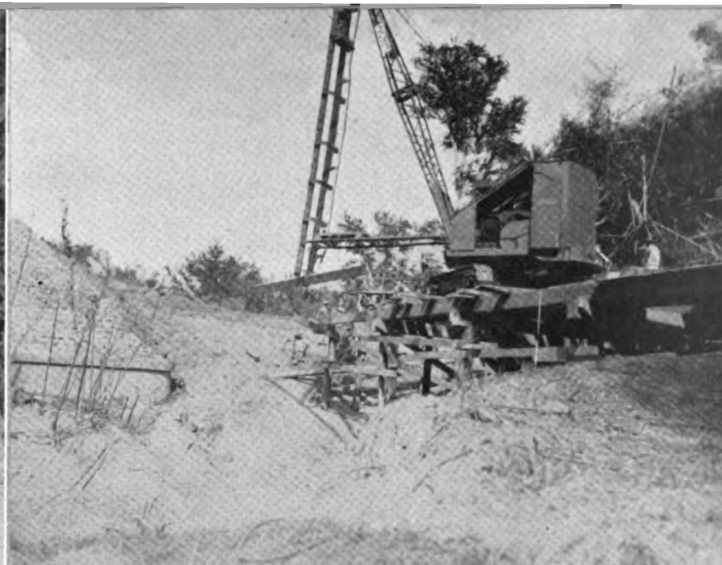


Monument to Filipino pioneers at Rosales.

9:00 o'clock Mass at Manaoag.

People going to market after Mass.





Company B Builds Temporary Pile Bridge Across Mitura River for Use as By-Pass.
Steel Bridge Later Reconstructed.





Good Friday ceremonies at Asingan.

Market Day at Asingan.



Mr. Castro entertains Battalion Medics.





Funeral services for 101 year old man. Wine, money, and hat are interred with deceased.



Washington's Birthday Anniversary parade.



These children entertained with folk dances.

What, no rice?



The honeymoon is about to start.





BAGITBAGIT RIVER BRIDGE

The Bagitbagit River Bridge was originally a five span reinforced concrete T-beam bridge 150 feet long. It was situated 3 miles east of Rosales, on the lateral Corps MSR, which connected the separate combat actions progressing up Route No. 3 to Baguio, and Route No. 5 to Balete Pass.

When captured, both abutment spans at either end of the concrete bridge had been blasted and dropped. The piers formerly supporting them were tipped crazily inward, twenty degrees from the vertical. From local information and evidence remaining in the stream bed, it was apparent that previous pile bridges had been inadequate and rehabilitation of at least that part of the concrete structure in midstream was advisable. The plan involved building two pile approach spans and raising and repairing the damaged pier.

The most interesting phase of the work was the restoration of the piers to the original capacity. When blown aslant, the piers developed lateral and diagonal cracks near the base and fractured at the point of intersection with the piles. The concrete at the top of the piles was crushed, completely exposing rods which were bent and distorted but not broken.

The construction procedure included jacking up the concrete T-beam slab, straightening the weakened piers, encasing the footings with concrete collars and reinforcing the points of fracture.





DS Bailey was constructed across gap in Pao River Bridge.



Filipino Sugar Mill near San Manuel—the syrup was made into basi.



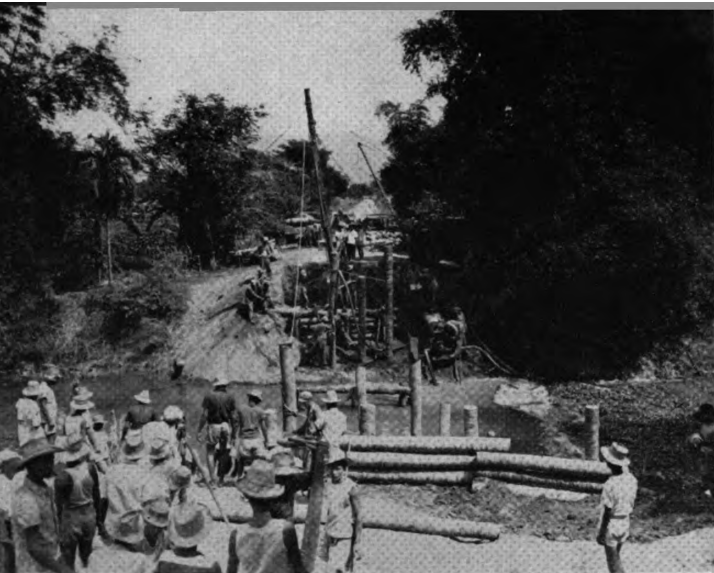
Brady and Jablonski with Jap flags.



Pruett and Sabbagh making bread in field kitchen near Binalonan.



Filipino chow line.



Jetting piling for bridge—a not so successful venture.
Ask the men who did it.



Pile bridge—Lagasit River.



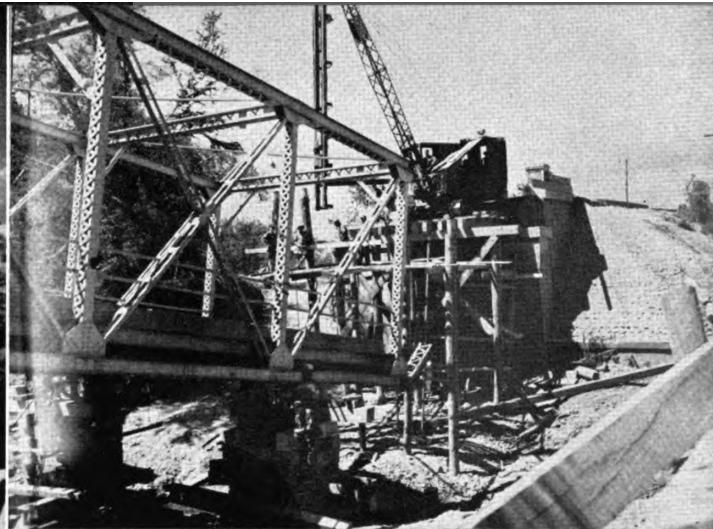
Double time on a pile bridge near Ago.

Company "A" repaired Arodagat River Bridge.



Queen Post Truss Bridge on Kennon Road.

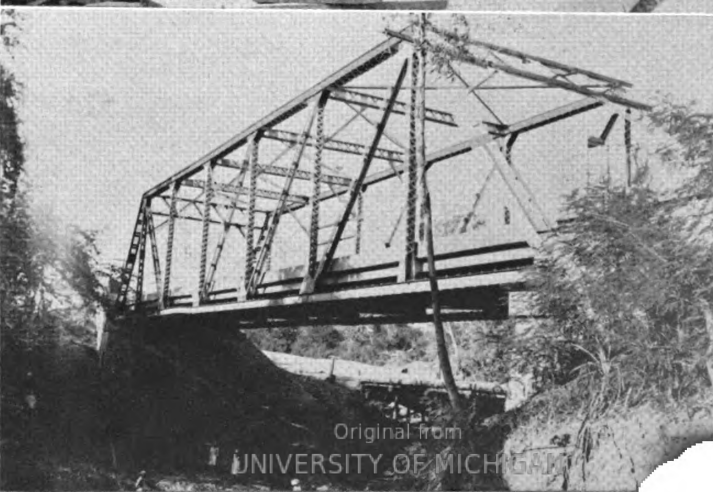
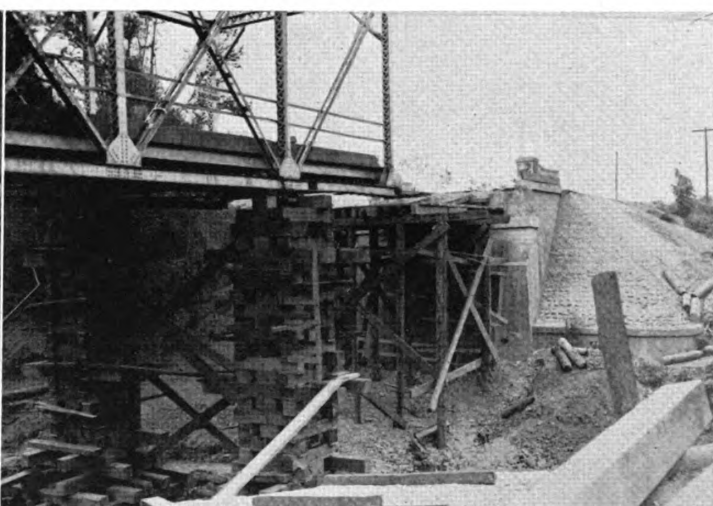
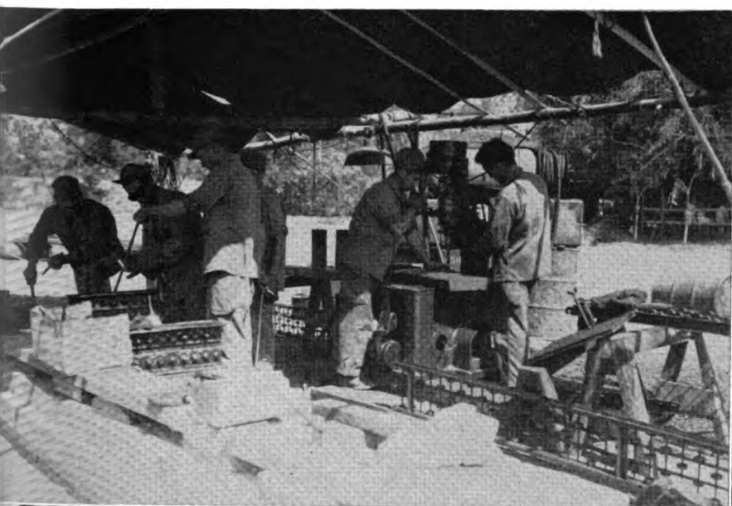




MITURA RIVER BRIDGE

This is a steel truss bridge of the through Pratt type across the Mitura River on Route 3 just north Urdaneta. It consists of seven panels with a total length of 120 feet, decked with a reinforced concrete floor slab. The bridge had been damaged by a demolition charge placed at the north abutment. This charge damaged the abutment and dropped that end of the bridge into the stream bed, breaking off and ruining the members of the end two panels. This left the bridge resting on the undamaged south abutment with the first five panels inclined at an angle of 20 degrees. In dropping, the bridge shifted 18 inches east of the center line.

The rehabilitation of this stream crossing was accomplished in three phases. First, was the construction of a temporary ford bypass on the downstream side of the original bridge. Second, a two way pile bridge ninety feet long was constructed on the other side of the original bridge. This pile bridge was originally intended to be the permanent stream crossing, however, a subsequent directive was received which called for rehabilitation of the original bridge.





I CORPS SAW MILL

On February 18, 1945, Company A moved from Santa Maria to Bitulok, a distance of 75 miles, to take over a sawmill just six miles from the east coast of Luzon. This move put Company A well ahead of all front line troops, so Infantry and Guerilla units were sent along for security.

Sabotage by the Japanese had caused extensive damage to the boiler and equipment. The mill was repaired and placed in full production within ten days after repairs were started. Operation of the mill also included a logging, reconstruction of logging roads and bridges, repair and maintenance of the road leading to the mill, and rehabilitation of the water system.

The area was guarded first by infantry and later by a company of Filipino guerrillas. The mill and camp area were both within the established perimeter. The entrances to the mill were booby trapped, but the booby-traps were discovered and detonated in time to prevent any casualties. While at Bitulok the unit accounted for ten Japs killed and two captured. It was here that Perkins was wounded by a Jap grenade, but not before he killed the Jap who threw it.

Bitulok is located in the mountains along the east coast seven miles from Dingalen Bay. The weather was ideal both for sleeping and working. Water for drinking and showers was cold and refreshing. The unit utilized the buildings in the mill area to a good purpose. The offices were used for administration by the unit, and the barracks building made a perfect combined kitchen and mess hall.

During six weeks time the Company put out a half million board feet of lumber, built 35 bridges, and put the mill at Dingalen Bay in operation. The unit developed its own logging areas and built roads to those areas.

To meet the need for clearspan bridges, ranging from 30 to 80 feet and utilizing native materials, various truss bridges were designed. Timber for these bridges came from these two mills, rehabilitated by Company A. The 510th Engineer Light Ponton Company hauled materials and prefabricated the bridges at their base camp.



Givens & Anderson bring in a big one.



Bitulok Logging Area.



Capt. Hilgendorf and Dumagats, a Negrito mountain tribe.



Dingalen Bay Stockpile.

Dingalen Bay Village—previously occupied by 10,000 Jap sailors.





Tom Romaine gets a big promotion.



Caratella.



Schoolteachers.



Sarantopoulous' playhouse at Rosales.



"You have dirty clothes, Sir?"



Statement of charges.

MANILA SOON AFTER LIBERATION



Legislative Building.



Pier 7.



Isaac Peral Avenue, Intramuros



City Hall



Escolta ruins.
Original from

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BUED RIVER BRIDGE →



This crossing was originally a steel truss bridge of five 81-foot spans over a wide river channel strewn with boulders. Of the original bridge three spans remained and these were badly damaged by artillery fire. The concrete pier which formerly supported the two destroyed trusses was repairable. The overall length of the two destroyed spans was 162'-6" which made it possible to obtain a perfect fit by using an H-20 bridge with reinforced diagonal members at the center where it was supported on the original pier.

The complete reconstruction of the bridge entailed replacing damaged truss members, replacing deck stringers, cutting down and repairing the concrete pier, patching shell holes in the bridge deck, and installing the H-20 spans.

It was necessary to relieve the bridge, a subdivided Warren truss, of any strain due to the sag in the upstream truss from the ruptured upper chord. The first job therefore was to jack up the truss below the break until both chords were level and parallel. The severed ends of the truss were then cut square, and a piece of chord of equal structural section salvaged from another bridge was welded in the gap. In the same span the truss on the downstream side had about an eight foot section of the bottom chord blown out. No jacking was necessary and it was sufficient to trim the frayed ends of the chord and weld in the required length of chord. This chord material was also salvaged from another bridge. In addition to replacing the damaged chords, further strengthening of the trusses was accomplished by replacing six diagonal members and two vertical members of the trusses which were severely damaged.

Simultaneously with the work of the structural steel crew and concrete crew, a third crew was employed in preparing abutment seats for the H-20 trusses, preparing an adequate seat by cutting down the center pier, and removing the debris of two temporary Japanese timber spans which had been demolished. Filipino labor was employed in the removing operations and care was taken to salvage all useable steel plates, bolts and sag-rods.

The four H-20 trusses were assembled using a P & H 20-ton truck crane to handle the panels and also to shove the trusses forward in launching. The entire truss was 162½ feet long over a center supporting concrete pier which divides it into two 81'-3" spans. After launching, the trusses were lowered onto the piers or abutment seats by use of a 20-ton crane.

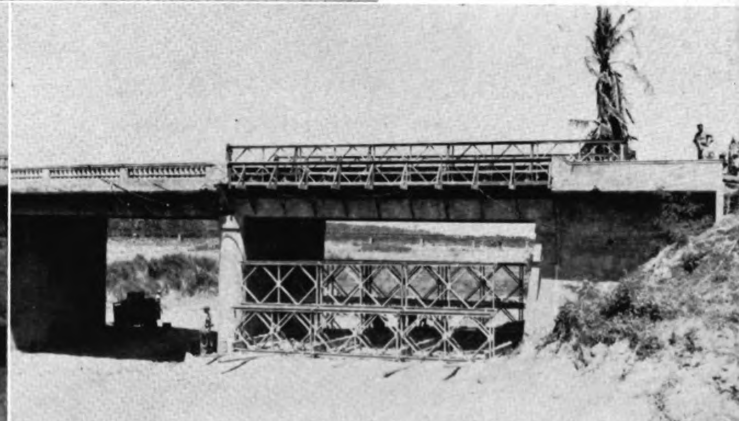
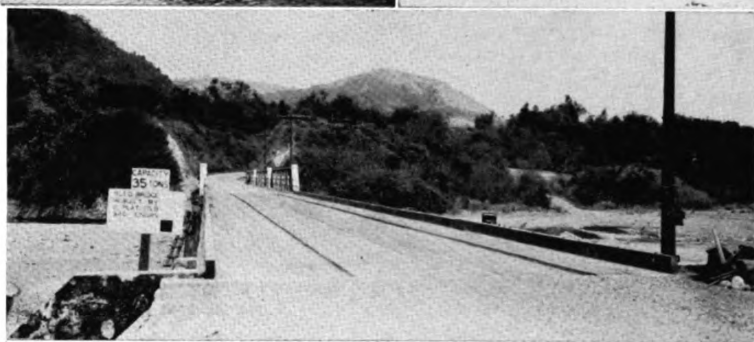
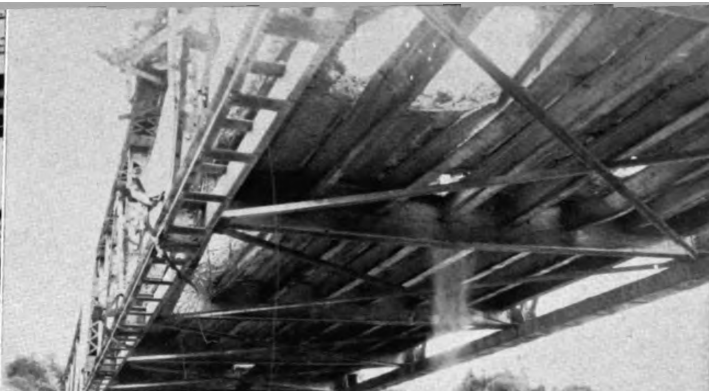
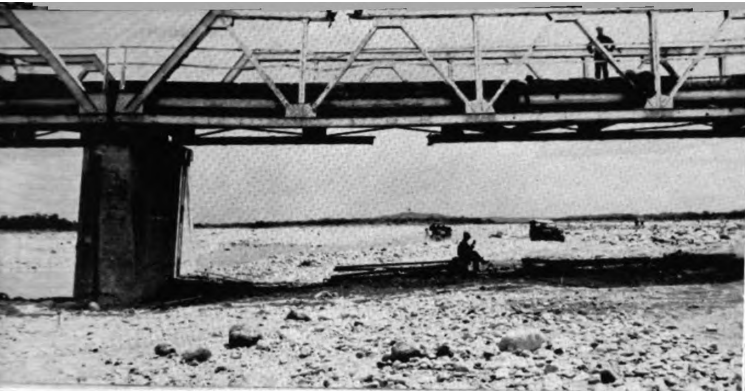
REPAIR OF ARINGAY RIVER BRIDGE →

April 7, Company "C" was assigned the repair of the 23 span, two lane, concrete stringer bridge over the Aringay River on Highway 3. The curtain wall and the east column of pier 16 were partly demolished; Span 18 had a ten foot crater in the west side of the roadway; and only the middle third of the north abutment span was intact. The bridge was kept open for traffic with a single-single Bailey carrying traffic over the north abutment span.

At pier 16 the broken concrete in the curtain wall was cleaned out and the damaged section of the wall was formed and poured. When this concrete had set the column was repaired. In both cases because the pier was in the river, concrete was chuted down to the forms from the mixer through a hole cut in the bridge deck.

The repair of span 18 was noteworthy for the hanging scaffolding, which supported the forms. The entire false work and forms were hung from the deck by one inch rods secured through the deck.

In repairing the damaged abutment span, the Bailey bridge was first raised so men could work under it removing shattered concrete, straightening, reinforcing rods and building forms. There was 100% salvage of the steel reinforcing rods in this span. Since the middle third of this span was intact, the west third was formed and poured, and seven days later these forms were removed and re-used on the east third of the span. Use was made of Bailey bridge parts for scaffolding to support the forms.





Camp near Agoo on Lingayen Gulf.



Chapel in Agoo Camp.



Harmon, Schmeir, & Lloyd receive Bronze Star Medal from Colonel Maxwell.



Lt. Aloff and his totem pole.



A few of the boys from Detroit.



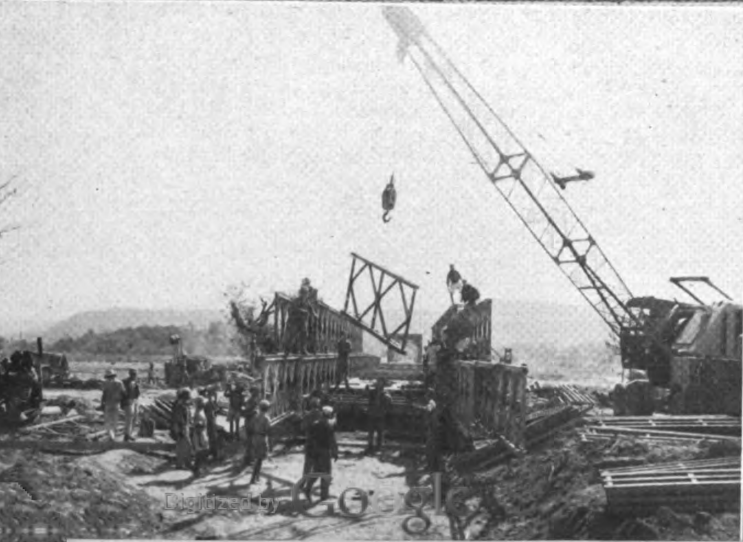
BAUANG RIVER BRIDGE

On Highway 9, five miles east of Naguilian, a Bailey bridge was constructed to replace a 190 ft. bridge which had been demolished. Because of the immediate tactical situation, it was necessary to put in a temporary bridge. After the divisional equipment had crossed, routine traffic was diverted to the ford and a more permanent bridge was installed.

For the first requirement, a 190 ft. triple Double Bailey bridge was supported on towers at the one-third points. The demolished bridge was cleared from the working site by a skillful demolition job which left the trusses lying on each side. The floor slab lay firmly bedded on the shallow channel and served as a foundation for the temporary intermediate piers. Both abutments were sound and by lowering the Bailey to these bearings set on the existing abutments, the bridge floor was brought to the roadway elevation thus dispensing with ramps at the approaches.

The intermediate piers are worthy of mention since they were made up of two bays of single Bailey Bridge, four stories high, and topped with timber cribbing to support the bridge.

After the divisional tactical movement had taken place, the bridge was closed to traffic. Then, near the north abutment, on the slack water side of the channel, a concrete pier was constructed and the bridge was changed to a 160 ft. clear span Triple Double Bailey, and a 30 ft. I-beam stringer bridge capable of carrying 35 tons. This reinforced concrete pier consisted of a 5'x4'x20' foundation, four columns 24 inches in diameter and 18 feet high and a 2'x4'x20' cap. The columns were formed using 24 inch corrugated steel culvert as forms. Since the column reinforcing consisted of 8-1" square rods, with $\frac{3}{8}$ inch round hoops on eight inch centers inside the corrugated forms, a very careful job of puddling and vibrating was necessary to prevent honeycombed concrete in these columns.



NAGUILIAN

This crossing was originally a five span steel truss bridge; each span being 151 feet 6 inches in length. Two spans at one end of the bridge were blown. A possible solution to this problem was the construction of a number of intermediate piers, then bridge these shorter spans with steel beams or wood trusses. This solution, however, wasn't acceptable in that it placed obstructions in the channel of a swiftly flowing stream with the consequent danger of a washout during the periods of high water. It was, therefore, decided to install a single 300 foot Bailey bridge across the two spans with the center bearing on the intermediate concrete pier which had been left intact. The work on this bridge was assigned to Company C.

The demolished trusses were dropped into the channel clear of the work by flame cutting the critical members. The south abutment and the two piers were

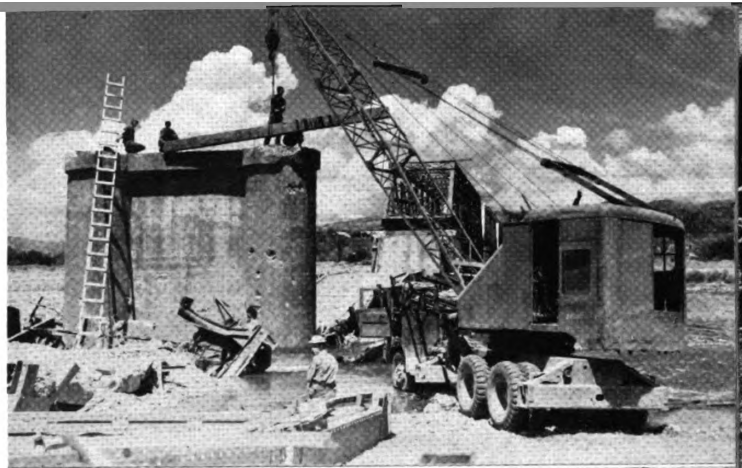


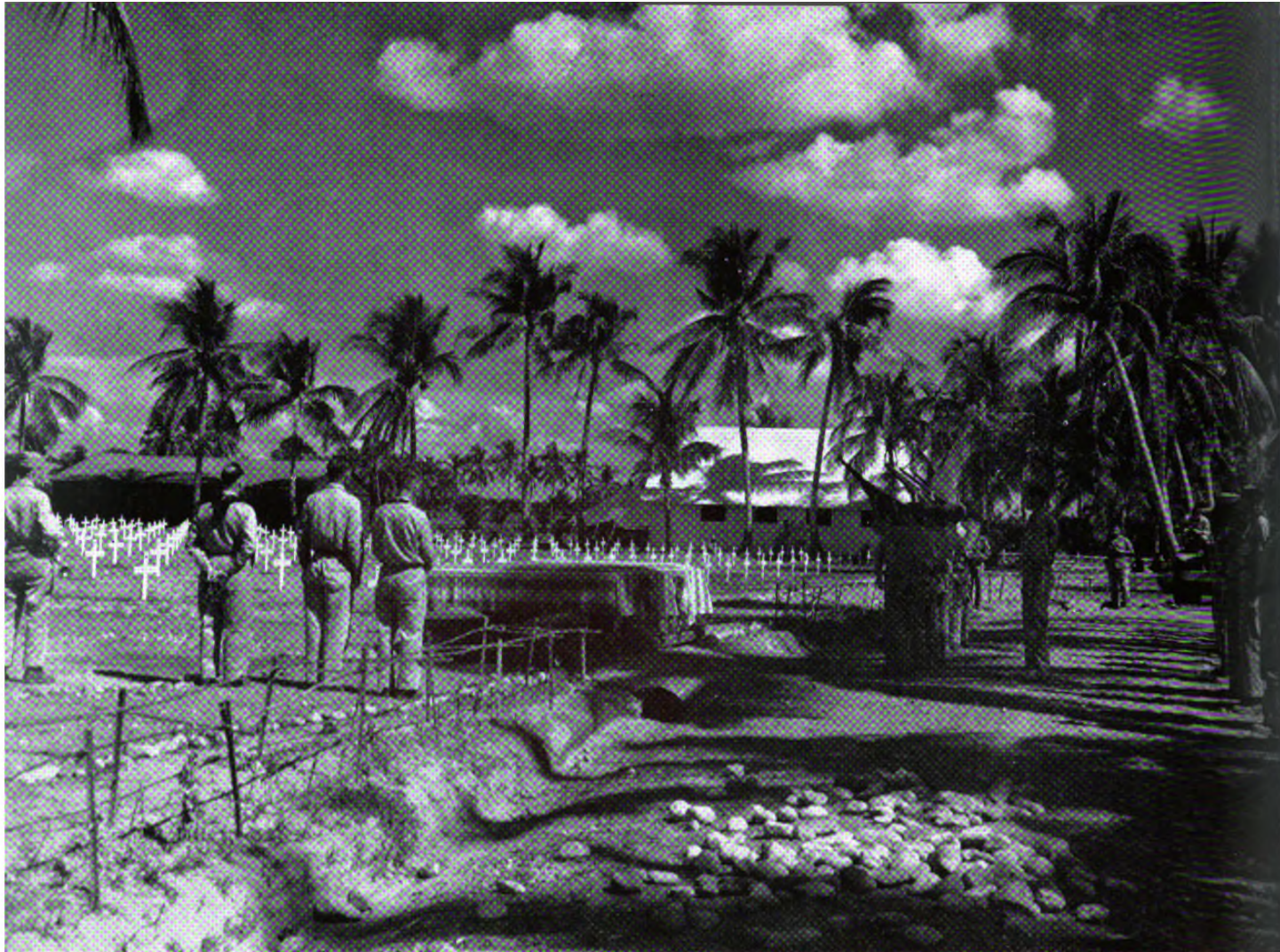
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RIVER BRIDGE

unharmd. The bridge was launched from the south abutment, passed over the center pier on rocking rollers and set over the bearings at the support where the existing bridge and Bailey bridge joined. The bridge fit perfectly into the existing pier shelves. Due to the depth of the floor system of the original bridge, it was necessary to crib up the pier seats to keep the treadway of the Bailey at roadway elevation. The rocking rollers on the intermediate pier which had been cribbed to their final elevation, remained in place to provide articulation as the permanent center bearing of the Bailey bridge.

Speed was paramount on this job as the bridge was needed for a vital tactical movement of combat troops. The target date was met after seven days of activity, most of this time was taken up in clearing the line for the Bailey bridge. Company C began work on this bridge, April 8, and completed it, April 15, 1945.



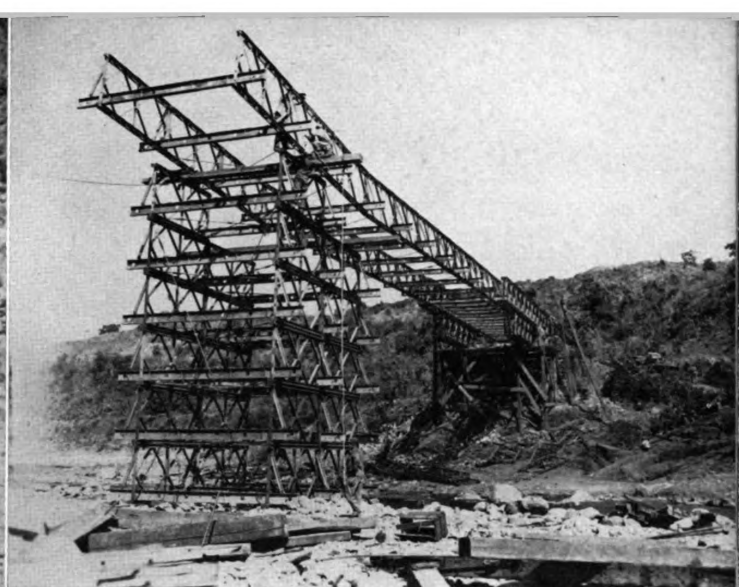


KILLED IN ACTION

While helping in the push toward Baguio, three members of the unit lost their lives when an enemy mine was set off. They were doing what is expected of every soldier, namely: their duty. But more than that, they gave their lives.

Funeral services were held for Corporal Amel T. Hockings, Technician 5th, Gerald C. Schmidt and Technician 5th, Julius Zupan; all of Company C. These men were laid to rest at Santa Barbara Cemetery, in Pangasinan Province, Luzon.





THE TRIPLE-TRIPLE BAILEY BRIDGE

On April 5, 1945, Company "B" began the erection of a 210 ft. triple story Bailey bridge over the Bued River on the Kennon Road, three miles north of Rosario. This is believed to be the heaviest and longest clearspan Bailey bridge constructed, to date, in the Pacific area.

The original suspension bridge had been destroyed during the early stages of Japanese conquest of the Philippines and only the south tower remained standing.

In order to construct a bridge of this length, it is necessary to construct an intermediate pier in the center of the span. This tower was made up of Bailey bridge parts two bays long and six stories high on the south half and five stories on the north or near half. Rocking rollers were placed on top of the pier to catch the launching nose as it came across. The job was purely routine until the nose rested on the temporary pier. The real job now was to attach the underslung panels as the bridge was moved out over the river. The method of attaching this bottom story was an interesting feature of the construction. The necessary parts were stocked at the base of the north abutment and hoisted to a working construction platform by two gin poles, one on either side. From this platform the men attached the panels to the middle story of the bridge. When the bridge had been launched to the point where the underslung panels rested on the rollers of the fifth story of the pier, the sixth story of



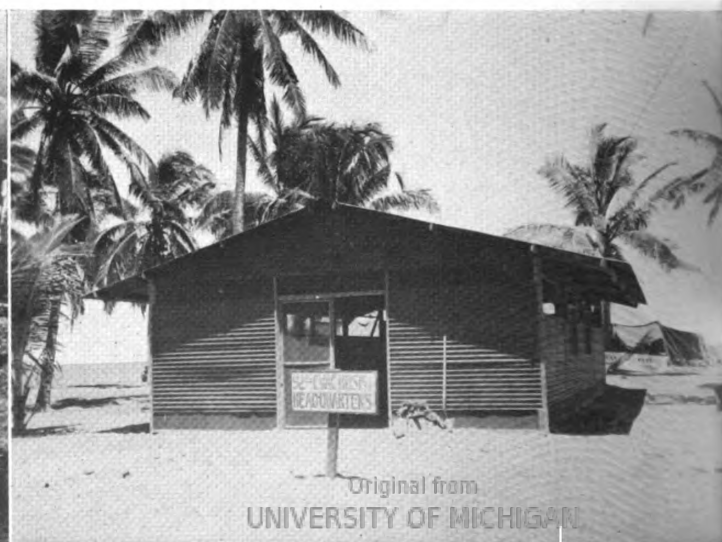
the pier was removed to allow the bridge to pass on its way rolling on the underslung panels. The launching continued until the triple-double completely spanned the gap. It was then jacked down into permanent abutment bearing.

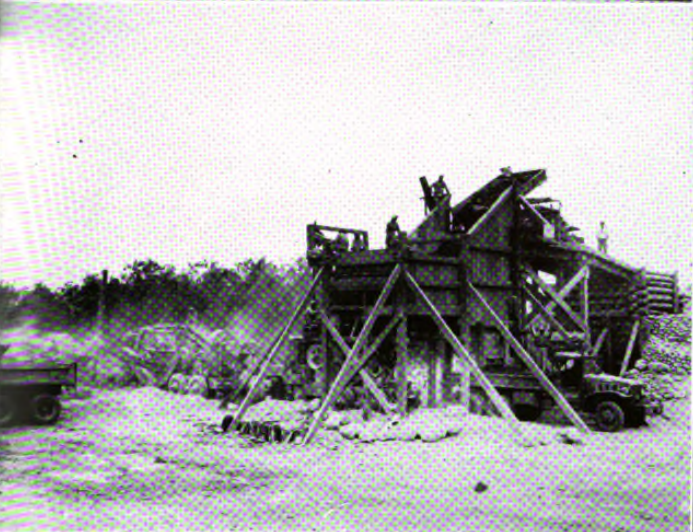
The temporary tower was left in place and the bridge jacked up to minimize the deflection and to facilitate placing of third story. When the tower was removed there was a small initial set, but no additional dead load deflection occurred later.



HOSPITAL AT CABA

In May, 1945, Company "C" was given the job of constructing a 500-bed Evacuation Hospital to be occupied by the 92nd Evacuation Hospital Unit. The site chosen for the hospital was along the beach of Lingayen Gulf just north of the town of Caba, La Union Province. The buildings were to be adapted from prefabricated tropical housing units. The wards were to be in hospital tents. The area was shaded by a grove of coconut palm trees, and with the sea breezes the location was a cool one. A surgery building, X-ray laboratory, administration building, mess halls, latrines, showers, and a garage were required. Due to the proximity of the ocean and the sandy nature of the ground, the location and digging of the well for water supply, the latrines, and the sump for waste water were accomplished very carefully. The resulting hospital was well constructed, and Company "C" was highly commended for its work by the Commanding Officer of the 92nd Evacuation Hospital Unit.





ASPHALTING ROUTE No. 3

For about two months from March 31, 1945, Company A was engaged in rebuilding and resurfacing 13 miles of road between Damortis and Aringay. To accomplish this task, it was necessary to build a detour through the rice paddies to divert the heavy traffic.

The road was first graded, watered, and rolled to provide a smooth subgrade. It was then oiled and spread with crushed stone in three layers. After much experimenting with the bitumen that was supplied for the job, a satisfactory design was established. It was necessary to construct an asphalt heating plant using two pontoon-cubes and gasburners designed by the H and S Company heavy equipment section.

Though the work proceeded without much trouble in the beginning, considerable difficulty was encountered during rainy weather. Ideal subgrade conditions could not be waited for because of the deadline for completion. However, the highway well served its purpose of carrying trucked supplies from the San Fernando supply base to points south and east.

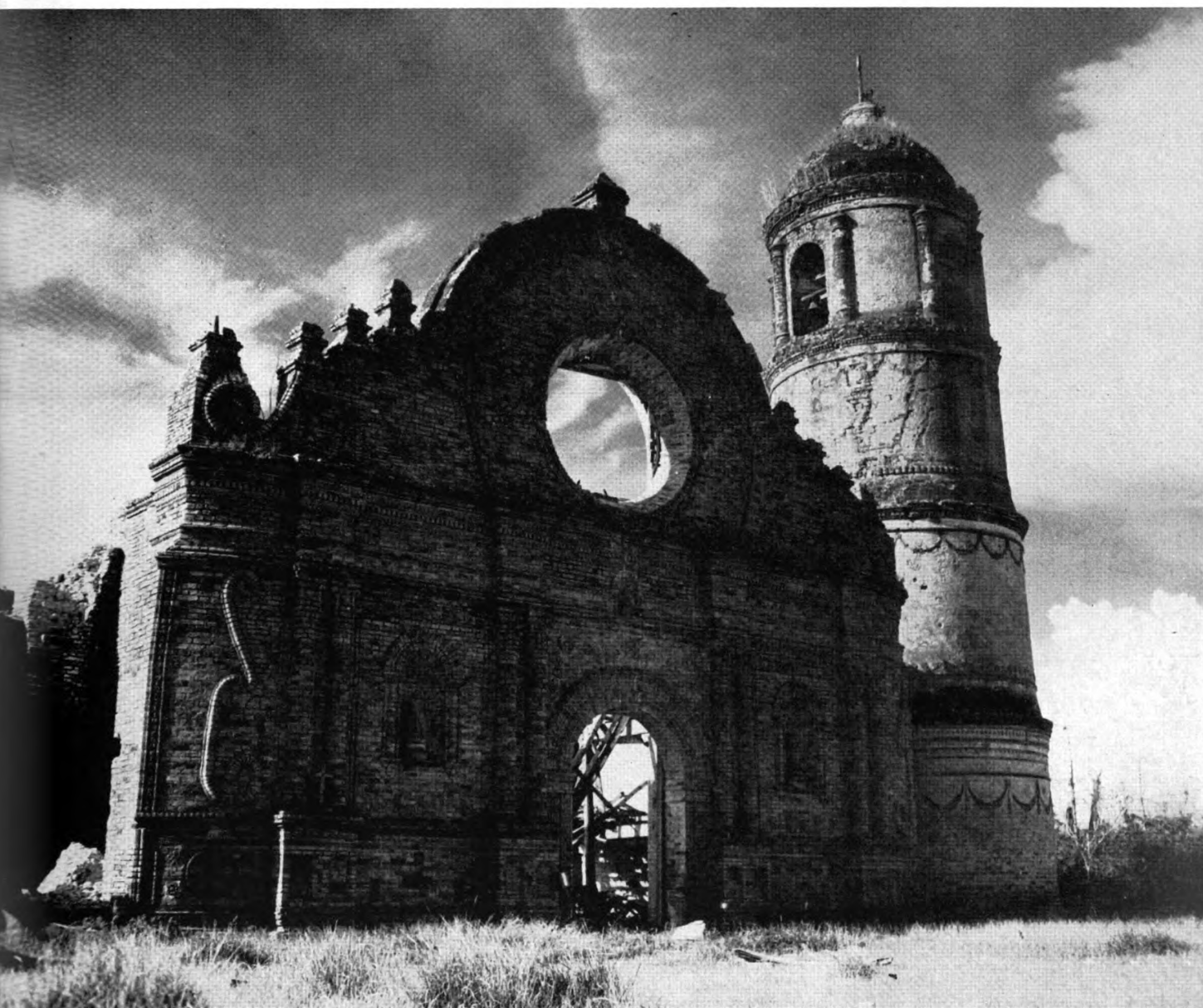




ROAD JUNCTION AT SANTA FE

The intersection of the Villa Verde Trail with Route 5 at the little town of Santa Fe represents the final objective and meeting place of the 32nd and the 25th Divisions. The 32nd fought an extremely bitter campaign against the Japs over the very rugged mountains traversed by the Villa Verde Trail. Since it was a trail and no more, it had to be widened to a road for use as a supply route as well as for use by combat vehicles. Two crews of men and bulldozers were taken from the 340th for cutting roadway out of the mountains right under the muzzles of the Jap guns. The dozer operators were fired at many times, and once a Jap ran at the tractor with a suicide lunge mine. Fortunately, the Infantry security squad got him first. The thick jungle, the frequent heavy rainstorms, the well concealed Jap snipers, and the occasional almost impregnable enemy defensive positions made the going very slow indeed along the Villa Verde Trail. In fact it took over two months to secure the twenty miles of trail which culminated at Santa Fe, in May, 1945.

The fighting done by the 25th Division over Balete Pass on Route 5 was just as tough, but the existence of the road for maintaining communication made the 25th's campaign much easier. As the 32nd outflanked the Jap positions along Route 5, the enemy pulled back, so the major conflict was that along the Villa Verde Trail. When the 32nd and the 25th reached the junction at Santa Fe, the 37th Division was sent in to carry on the advance through the Cagayan Valley. The 37th was made into a mobile striking force with plenty of tanks and armored vehicles, and the Japs were pushed back along Route 5 at the rate of 10 miles a day. By the end of June, 1945, the 37th had reached Aparri at the northern tip of Luzon. During June and part of July, the 340th was moved over into the Cagayan Valley, and there built many bridges and maintained the road.



IN THE CAGAYAN VALLEY

Some of the first Catholic priests that came from Spain to educate the heathen Filipinos went into the Cagayan Valley. In the course of the succeeding three hundred years, many imposing churches were constructed, used, and abandoned by the priests and their laboring parishoners. The churches fell in and were grown over by the fertile grasses and vines. These ecclesiastical relics were mouldering quietly away as the flood of war passed by.

The churches were silent witnesses to the Cagayan Valley campaign begun in June, 1945 immediately after cracking the enemy defenses in Balete Pass. Company "A" of the 340th Engineers was the first group to enter this campaign for the unit. Much of the same type of terrain as in the coastal valley and foothills was encountered. The campaign started with a rush and continued at a high rate of speed until the entire length of Route 5 was secured. A total of 235 miles of road was taken in 30 days. It was necessary to repair or completely rebuild ninety-five bridges in order to keep open the MSR behind the advancing 37th Division.

June 20, Company "B" moved from Baguio to Ilagan where they built several bridges and a landing strip, and then later to Tuguegarao on Route 5. July 21 Company "B" moved back to Route 11 near Camp 30, Mountain Province.

June 19, Headquarters moved from Agoo to Santiago, Isabela, and eight days later to Tumauni on Route 5. July 21, 1945, Headquarters moved from the Valley to Camp John Hay, Baguio.



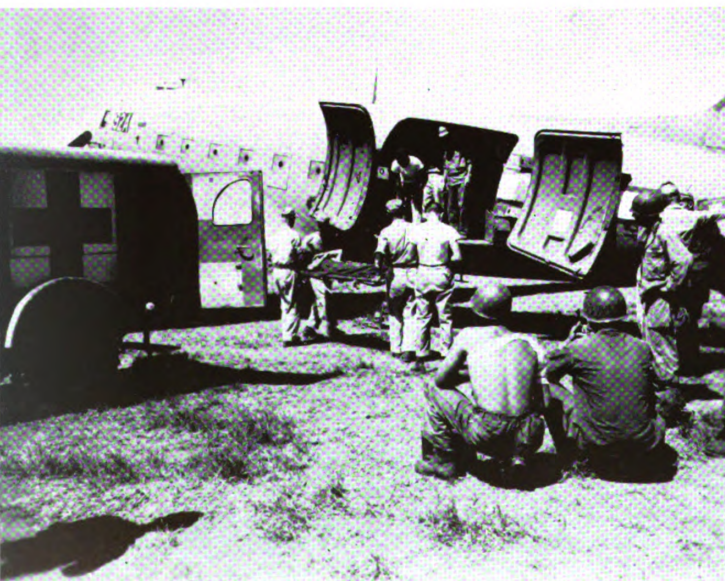
Hash in the Cagayan prepared by Borg, Lynch, York, Alexander, and Giglio.

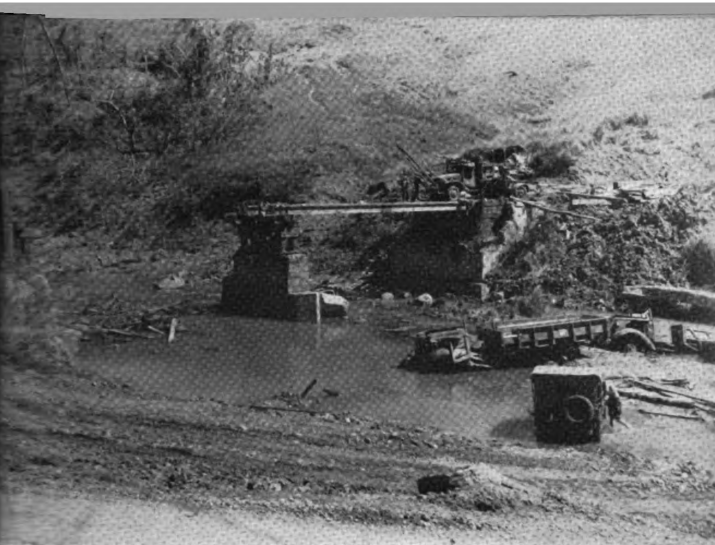


Yes, Mrs. Schure, he did wash behind his ears.



Feeding hungry Filipinos at Santiago. Skillings and Force supervise.

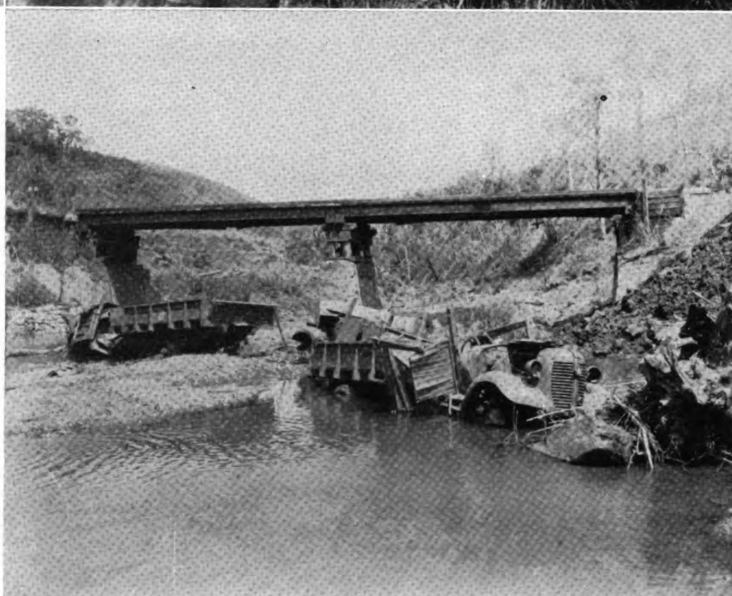




WELDED STEEL STRINGER BRIDGE

This bridge was near the second camp site of Company A during the Cagayan Valley push. It was here that a fair size convoy of Japanese were trapped and finally wiped out. The 115 Engineers were drawn into this battle. As the Japanese attempted a crossing at this ford with their trucks, carts, men and horses, they were spotted and fired on with artillery. In the confusion that resulted from the artillery, air attack, and the fact that the crossing was jammed, the Japanese had no chance whatsoever.

As the 37th Division moved forward the 340th moved into position to repair the bridge. To repair the bridge, Company A welded steel stringers and went on to complete the job in the usual manner. One could hardly call the conditions under which the men worked rosy, since the bodies from the battle a few hours past were still strewn throughout the area.



CONCRETE ARCH BRIDGE





BRIDGE No. 979-Hwy. 5

The problem of a river crossing to be needed during the season of high water was solved by use of a double-double Bailey bridge supplemented with an intermediate pile bent pier and a standard 2-span timber pile bridge at the south end. The original bridge was destroyed by our airforce during the Jap's hasty retreat up the Route 5. It consisted of two concrete abutments with two simple steel stringer approach spans on concrete piers; the main span was a through-deck steel span.

When bombed out, the main span and the south approach span were demolished along with the main pier on the south.

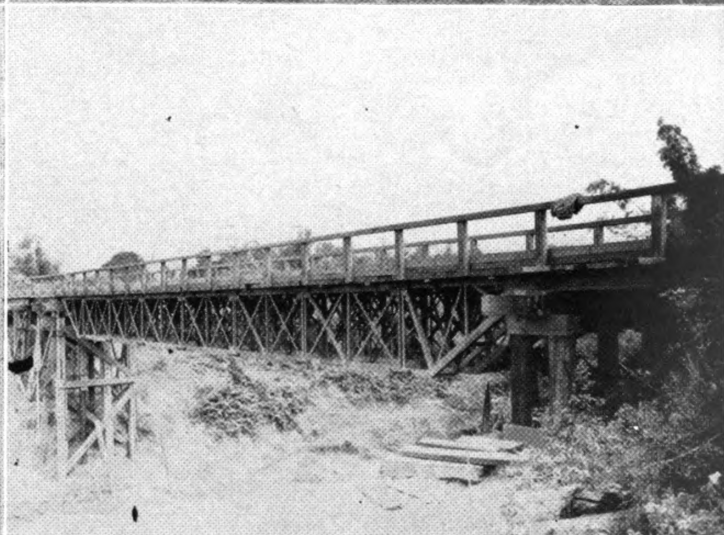
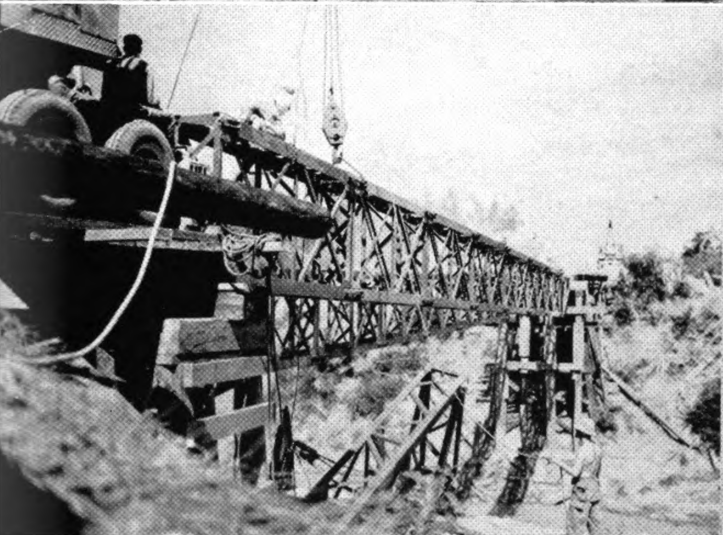
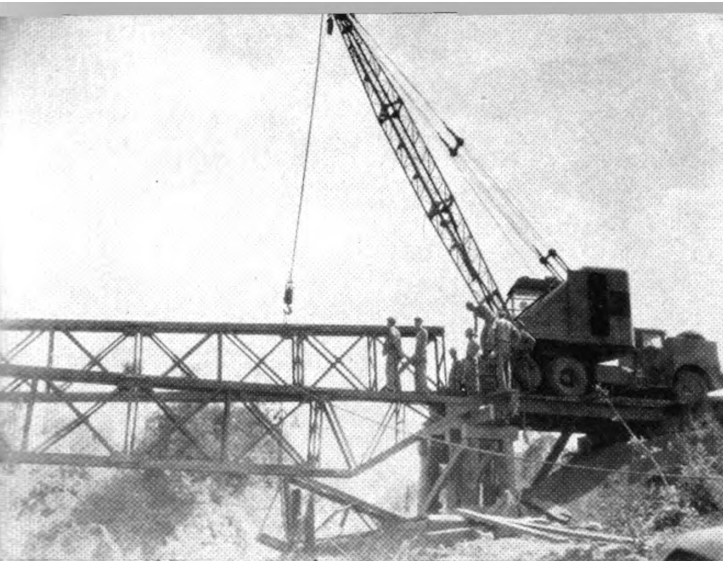
The pile driving crew replaced this approach and put in a twelve-pile bent in place of the wrecked concrete pier. Coincident with this work, the old steel main span was cut up with torches and dragged from the river bed with a bulldozer. Then a Bailey bridge was launched to complete the job. During construction, two spans of pneumatic pontoon treadway were used for a temporary crossing.

Company A did the work and employed a crew of Filipinos to supplement their own forces.

This crossing and those constructed by Company B, shown on the following page, were important because they kept traffic moving over the only MSR up the Cagayan Valley for the fast moving 37th Division.

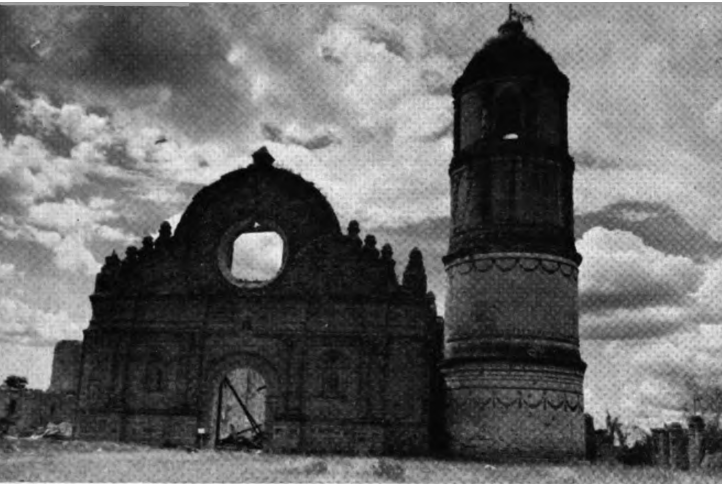
H-20 BRIDGES →

During the advance down the Cagayan Valley, the principal factor influencing bridge design, other than that imposed by terrain, was the supply of materials. Due to the shortage of adequate trailers, it was impossible to build up a stock of long piles to be used in rebuilding the many timber bridges which had been burned out by the retreating enemy. It was necessary to provide long, clear spans over several of the deeper stream channels. Since the required spans were less than 100 feet, and an adequate supply of H-20 bridging was available, the decision was made to use the prefabricated steel bridging for four of the bridges.



Four H-20 bridges were constructed between Cagayan and Ilagan: two by the 186th Engineers and two by the 340th Engineers. Three of these were 87½ foot, 2 girder spans, while the fourth was a 100 foot, 3 girder span. All four stream crossings presented the same problem; the channels were too deep to permit use of the limited number of short piles available. It was therefore decided to use the short piles in the construction of approach spans at each end of the bridges, and then to span the deep center portions of the channels with H-20. The overall lengths of the four completed bridges were as follows: 120', 160', 165' and 175'.





Ruins of ancient church at Tumauni, built in 1705 by Spaniards.



S-3 in the field.



1st Sgt. Maloney—there's one in every outfit.



Sarantopoulos pays off.

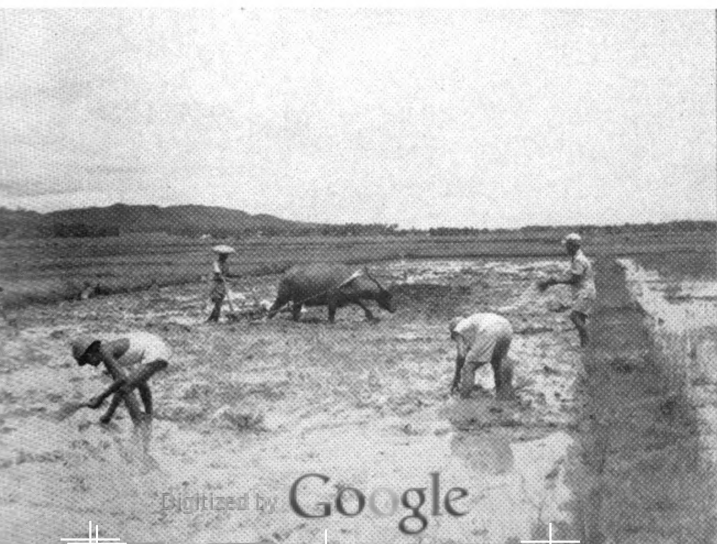


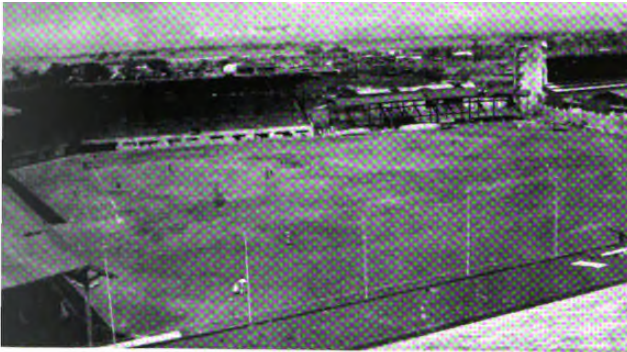
Major Gribble leaves to assume command of 118th Engineer Construction Battalion.

It's Fraga—not a Jap.



RICE PLANTING ON LUZON





Rizal Stadium—Manila Bay in background.



Ruins in Manila Business District.



Round-the-world speed test flight.



Rizal Colosseum.

MANILA IN AUGUST



Manila street scenes.



Main Intersection—Rizal & Ascaraga



The once fashionable shopping district.

Original from

UNIVERSITY OF MICHIGAN



Bringing produce to market via the Pasig River Canal.



Post Office



Shore leave.



Modern architecture in Manila.



Heavy traffic on Quezon Blvd.



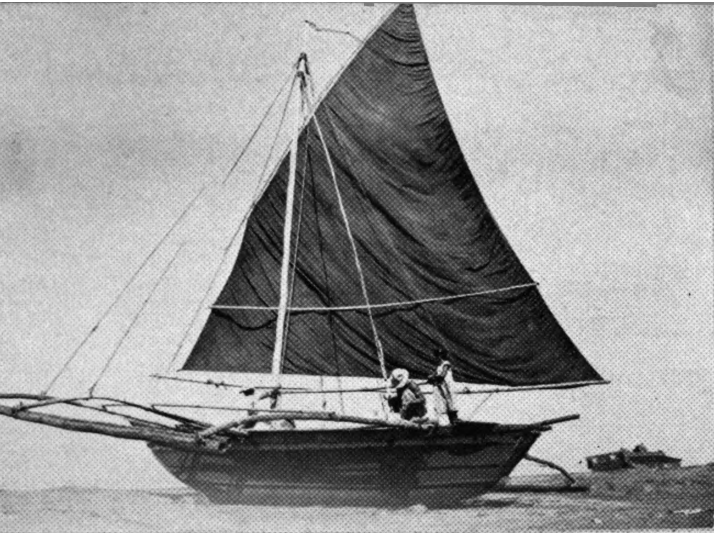
Chinese architecture.



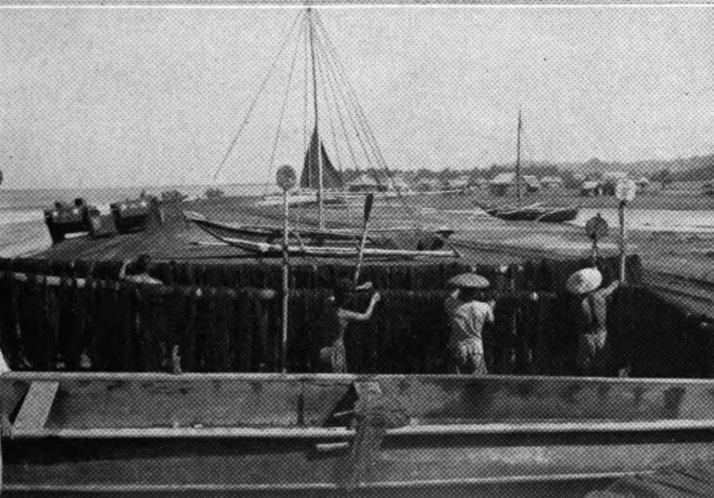
Modern home on Taft Avenue.



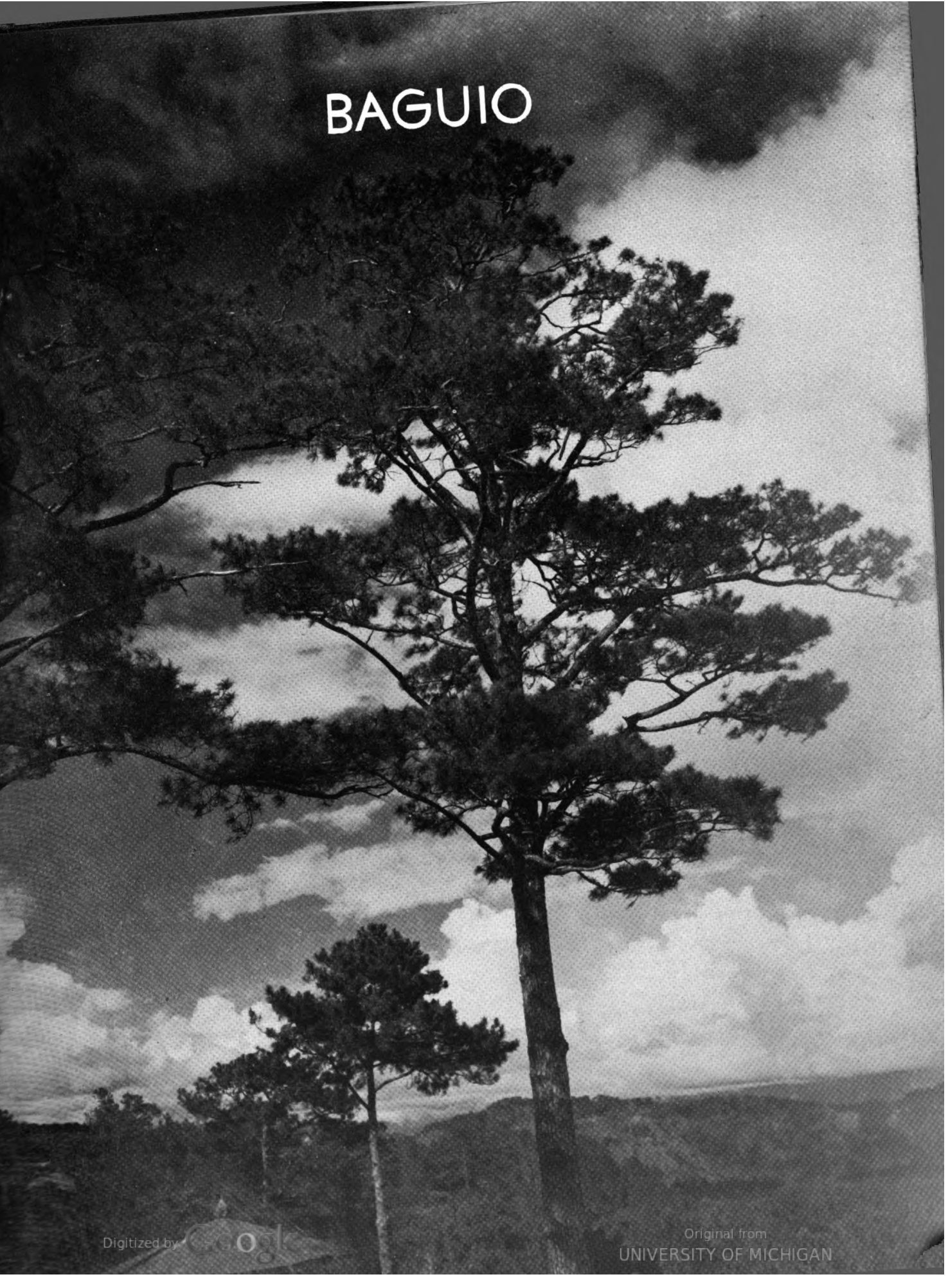
B-29 at Nichols Field.
UNIVERSITY OF MICHIGAN



A FISHERMAN'S DAY



BAGUIO





Looking up Session Road.

BAGUIO

Baguio is the general name of a wide rolling plateau, surrounded by towering peaks of which the city of Baguio is the center. The area is covered with virgin pine forests which impart to the air an invigorating and healthful tonic fragrance.

Baguio is the entrance from the west to the Mountain Province. This province contains hundreds of mountain peaks and deep valleys. The broken character of its topography provides scenery that is unequalled anywhere. The highest peak is Mt. Pulog (elevation 9613 feet). Bontoc is the capital of the Mountain Province. Of course Baguio is the gateway to this enchanting scenic wonderland, 5000 feet above the sea.

The tribes inhabiting the vastness of Mountain Province present interesting studies of primitive life, unique ceremonies and tribal customs, but little touched by modern influence. The fierce head-hunting practices have been largely checked but the life in the villages hidden in the hills goes on. The hill people are generally known as Igorots. They are said to be direct descendants of the earliest Malay invaders of the Philippine Islands. These people have occupied this territory for at least 200 years. Each of these tribes has its own distinct dress, social and tribal customs, speech, ceremonies, modes of living and working. The Mountain Province presents one of the world's most interesting vacation lands combining strange, primitive people with scenic masterpieces, marvelous rice terraces, thundering waterfalls and giant forests.

Visitors from the United States in recent months have found here new ethnological studies of types found nowhere else, which are absorbing in interest and variety. It is nothing to see little groups of these charming people standing beside the road in their picturesque rags, chattering happily about the inflated cost of rice, or some new scheme for gouging the tourists.

The city of Baguio is built on a hilly plateau at an elevation of 5000 feet. From Manila one would pass through green rice fields, coconut groves, primitive palm-leaf huts, and Tagalog towns; past sugar mills and mud puddles where wide-horned carabao wallow all submerged except for their sullen, brooding blue faces. Nearing Baguio travelers run up against steep foothills where the roads leading to this city in the sky become dizzy zigzags; here, in a few minutes, you quit the hot sticky lowlands air for the cool Baguio breezes. A turn through the Alps or the Colorado Rockies might be comparable.

Baguio itself sprawls along miles of winding pine-shaded walks and drives. Here is the summer home of the President of the Philippine Commonwealth, a luxurious mansion for the American High Commissioner, hotels, barracks, golf courses, and the hot weather homes of many Manila families. Furthermore, several lavish stockades stud this recently war-flattened paradise. Best Baguio sight is the market, when all the reformed head-hunters flock in. What glutinous heaps of things to eat. How symbolic of tropic fecundity this market is! Trees, plants, vines, hens, every sow and cow seem to be pregnant, even the stork seems to be at his gay, blithe best.

Only you lucky people who have been there can truly appreciate this extravaganza.



St. Louis Cathedral.



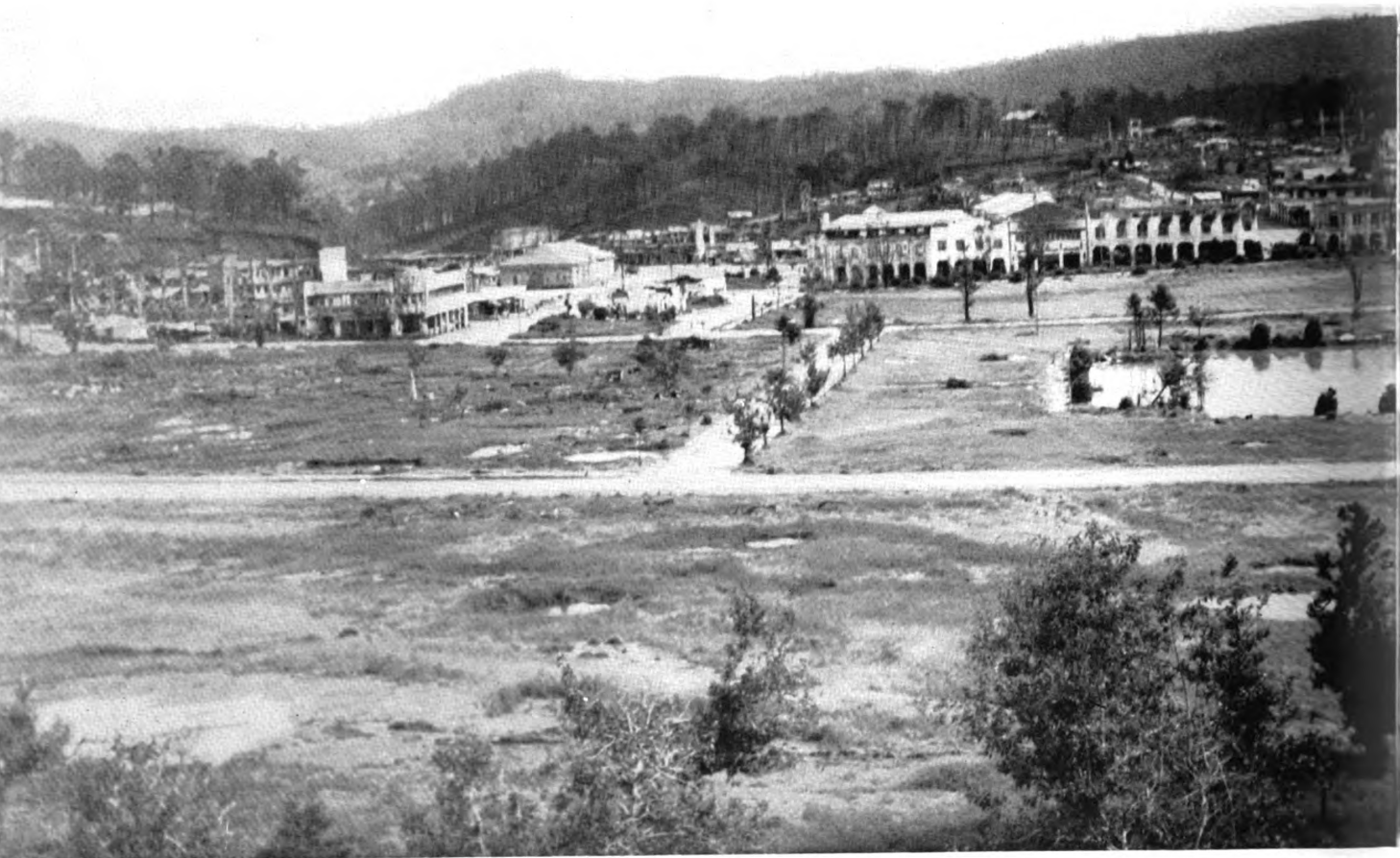
H/S Camp — Building is former officers' dormitory at Camp John Hay.



Where to?

Bomb damaged buildings in Baguio.



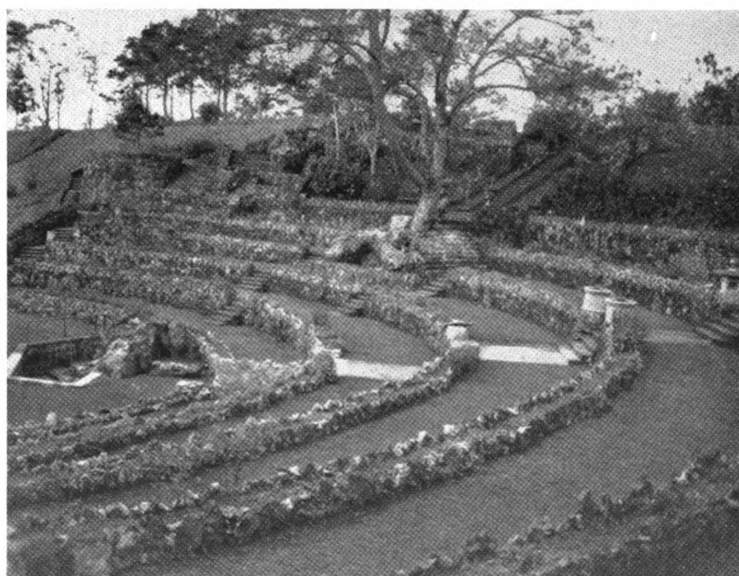


Baguio Across





Burnham Park

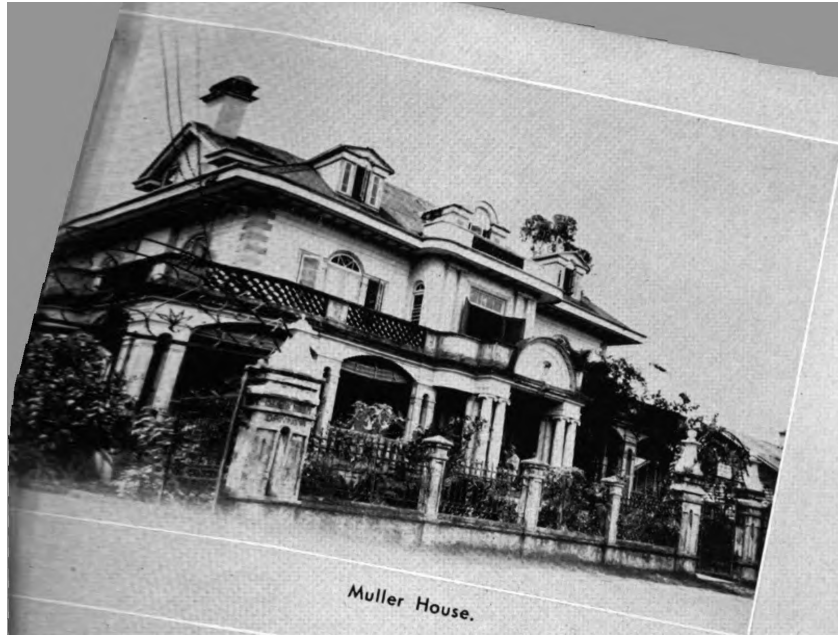




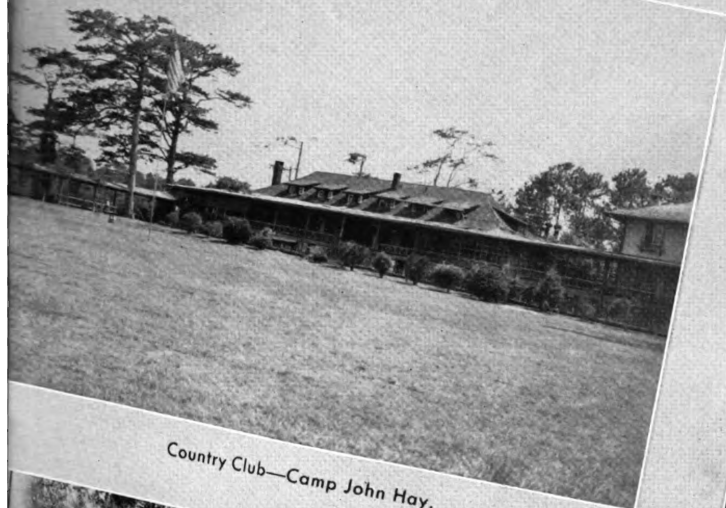
Another shot of the St. Louis Cathedral.

Penafrancia Building—what's left of it anyway.





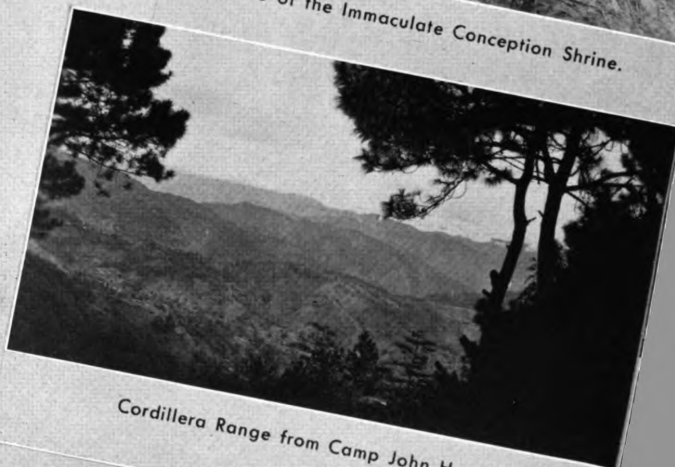
Muller House.



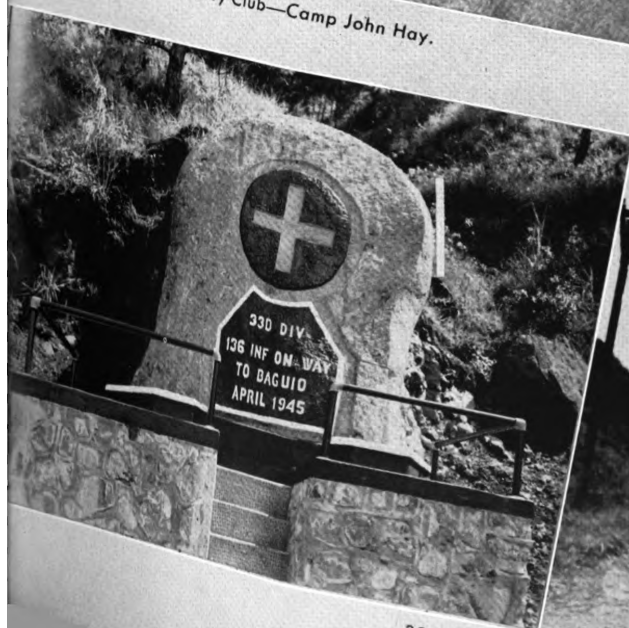
Country Club—Camp John Hay.



Grotto of the Immaculate Conception Shrine.



Cordillera Range from Camp John Hay.



33rd Division monuments.





Mountain girls from Senipsip Gate.

Baby goes to Baguio Market—For sale were: Green Onions, Strawberries, Cucumbers, Pigs, Bananas, Dogs, Asparagus, Corn, Avocados, Guayas, Rice, Fish, etc. etc.



Igorot women do the carrying.

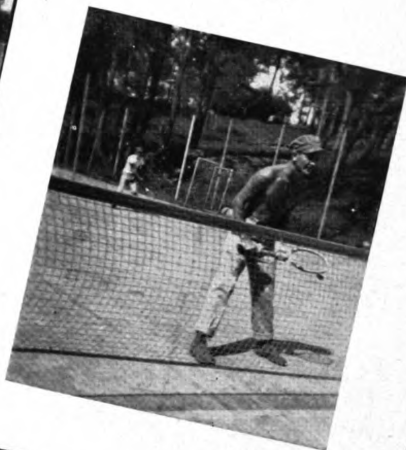
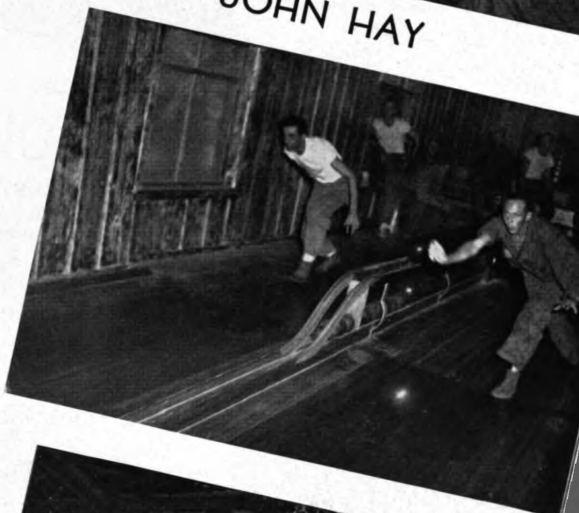
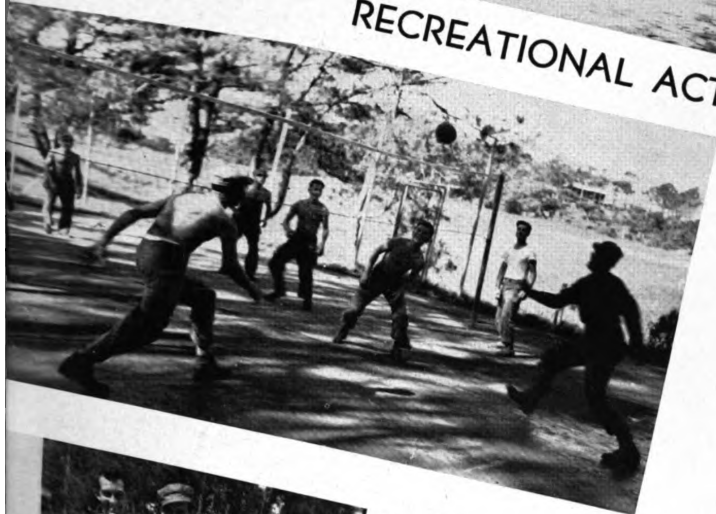
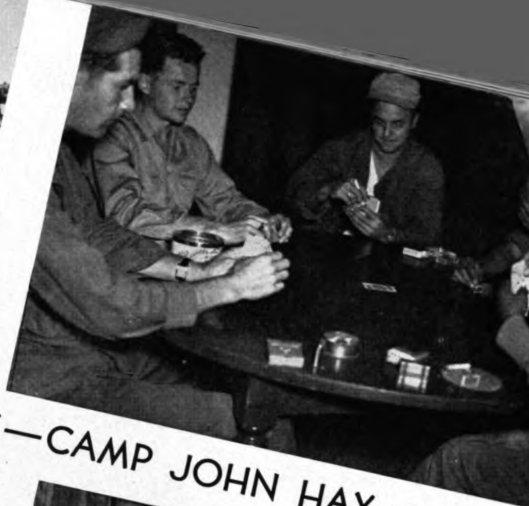
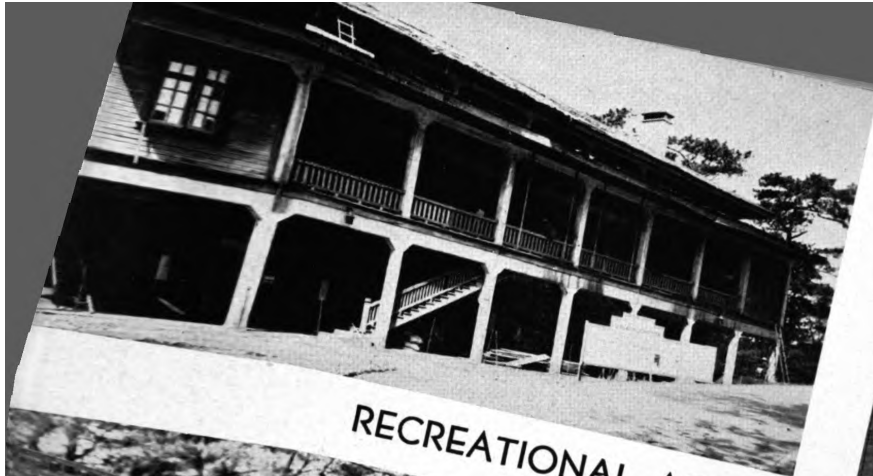


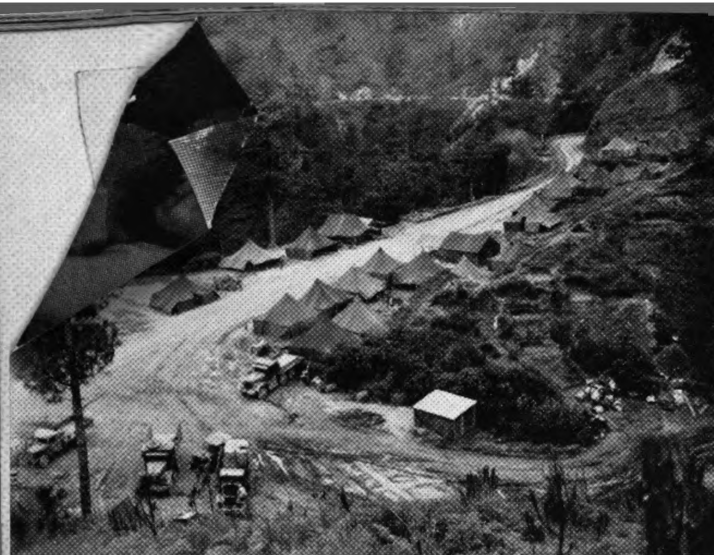
Igorot guerillas and mother.



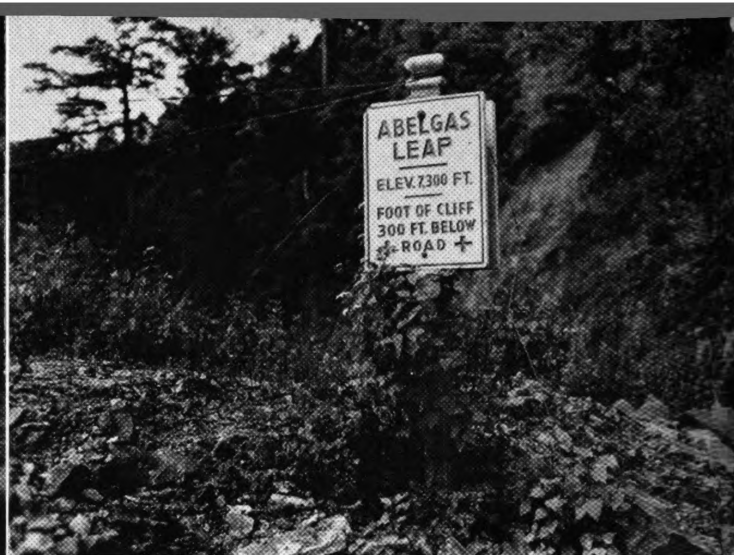
Kay Kyser Show at Camp John Hay Recreation Hall.

RECREATIONAL ACTIVITIES—CAMP JOHN HAY





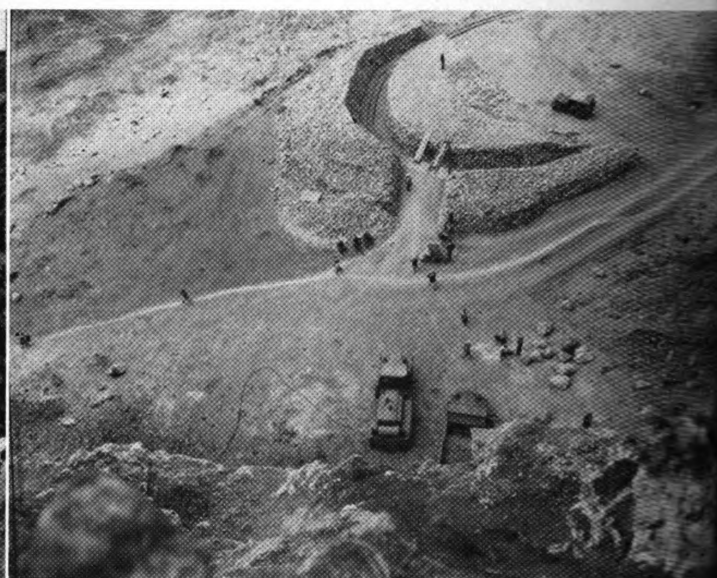
Company "B" Camp at KP 30 on Rt. 11.



Abelgas was a disappointed lover.

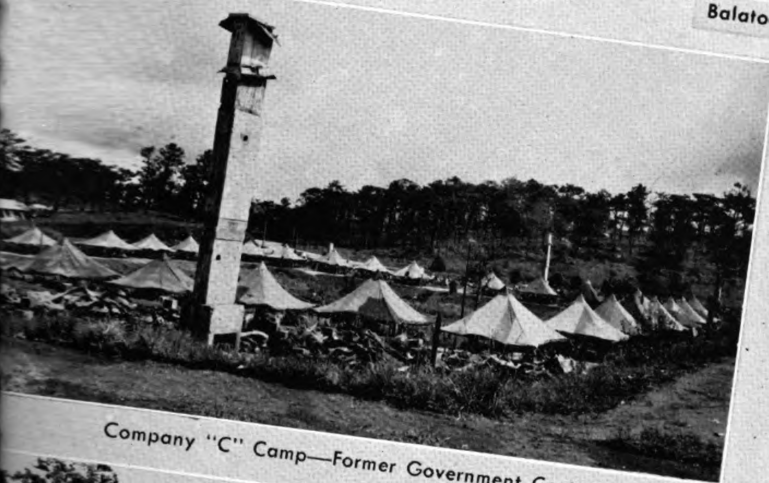
BAGUIO QUARRY

This quarry was operated and supervised by members of Company A with men of Headquarters Company assisting. Approximately 90 per cent of the labor was done by Filipinos. Blasting was handled by men from the 340th. The amount of gravel usually put out during this time at the Baguio quarry was between 8 and ten yards per hour.





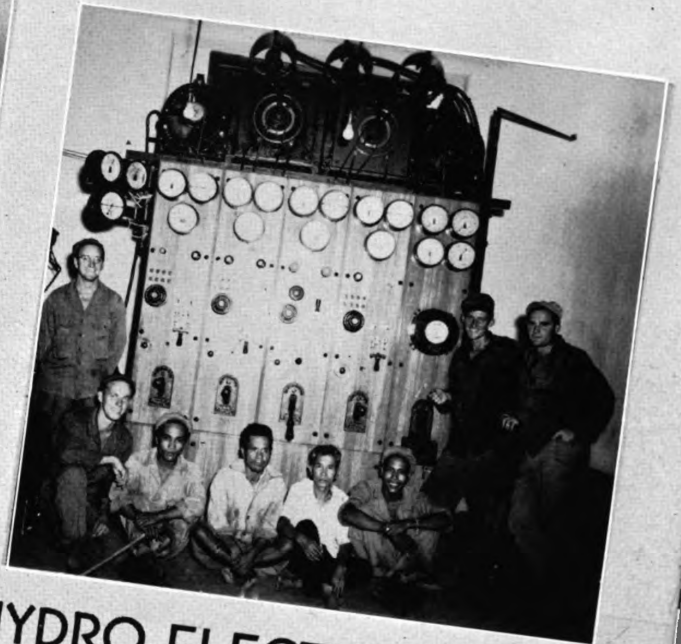
Balatoc Mine.



Company "C" Camp—Former Government Center.

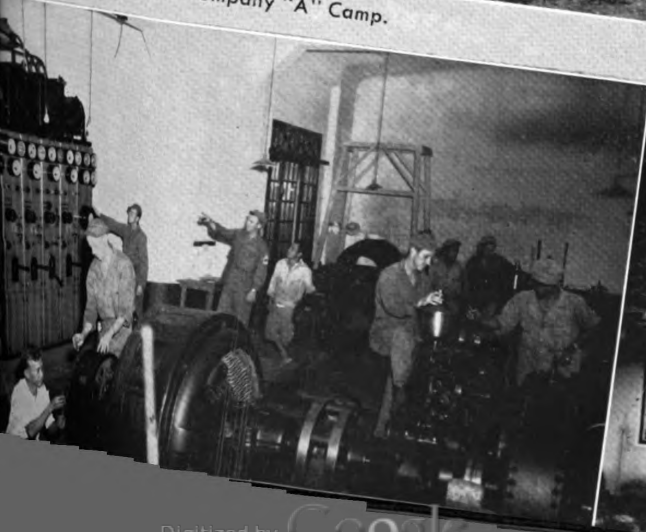


Company "A" Camp.



HYDRO-ELECTRIC PLANT

This plant had been damaged somewhat by the Japanese just before the entrance of American forces into Baguio. It was originally to supply power for Camp John Hay. The job of repairing this Hydro plant was given to the 340th Engineers. Not only the plant needed repairs, but the entire network of pipe-lines and wire throughout the area around Baguio were in dire need of repair or replacements. After all was in order, the plant served all of Camp John Hay. Two other city plants could have been put in operation if replacement parts and wire could have been obtained.





High Commissioner's House — Scene of Surrender.

THE SURRENDER OF YAMASHITA

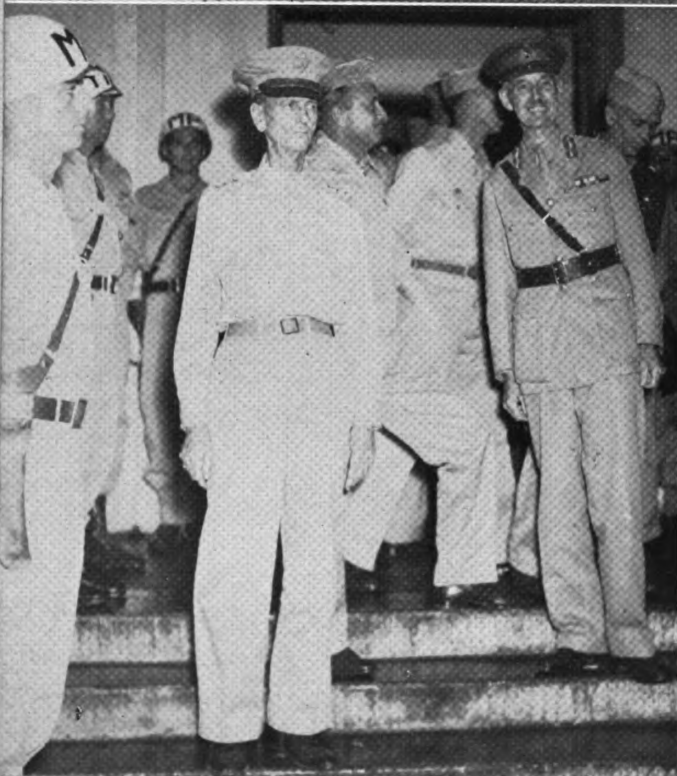
On September 2, 1945, General Yamashita surrendered himself and the entire Japanese force in the Philippine Islands, to Major General Leavey, representing General MacArthur who was then in Japan for the signing of the final and official capitulation there.

The instrument of surrender was signed in the High Commissioner's mansion at Baguio, the summer capital of the Philippines, high in the mountains of central Luzon, only a short distance from Camp John Hay where the 340th was stationed at that time. It was here that the turn-about of those top Generals, Wainwright, Percival and Yamashita took place.

The Japanese General spoke little. He spoke only to his interpreter concerning the paper before him, saying, "I am," to the question of General Leavey, "Are you ready to sign?" He seemed unperturbed throughout.

After General Leavey accepted the surrender, the Japanese were escorted from the room, prisoners of war. Wainwright and Percival were the first to leave the large white mansion, as shown in the picture above.

The "Tiger" came out a few hours later and waited to be delivered to the Bilibid prison, Manila. This historic event will be long remembered by members of the 340th Engineers.



Generals Wainwright and Percival.

The fangless "Tiger of Malaya"

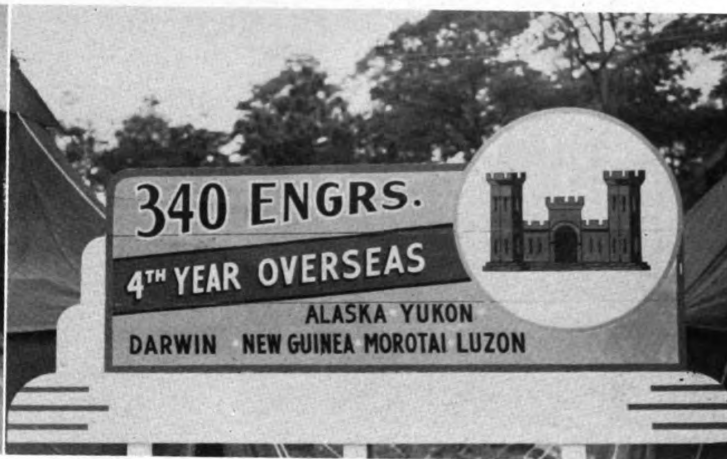




Going home on points.



Company "A" goes on a hike.



Propaganda.



Levine & "Count" Richlovsky in "The Phantom."



Plague shots.

JAPANESE

The Emperor's Imperial Rescript of 16 August 1946 was the official word from the prostrate Japanese nation that it was ready to say, "Uncle—Uncle Sam, you win." Thereafter, the Jap soldiers began to surrender in large numbers.

Most of the remaining Nips on Luzon were holed up in the mountain ranges north of Baguio. When they surrendered to the American Forces and Filipino Guerrillas, they weren't given the comforts of home, but were stripped of their guns and sabers and cameras and then marched in to Baguio, a distance of 60 miles, although many were brought in by truck. Occasionally, it rained



PRISONERS

and their feet got wet, but their hike was like a walk around the block compared to the march the surrendered American soldiers had undergone three years before.

A large stockade was needed in Baguio in a hurry to hold the prisoners, and it was built by the 340th in three days. Platoons from Companies A, B, and C worked in shifts to dig 1200 post holes, put up posts and barbed wire for a mile-long, eight-foot fence, and set up guard boxes and flood lights. A total of 20,000 Japanese were staged for separation from active duty there.

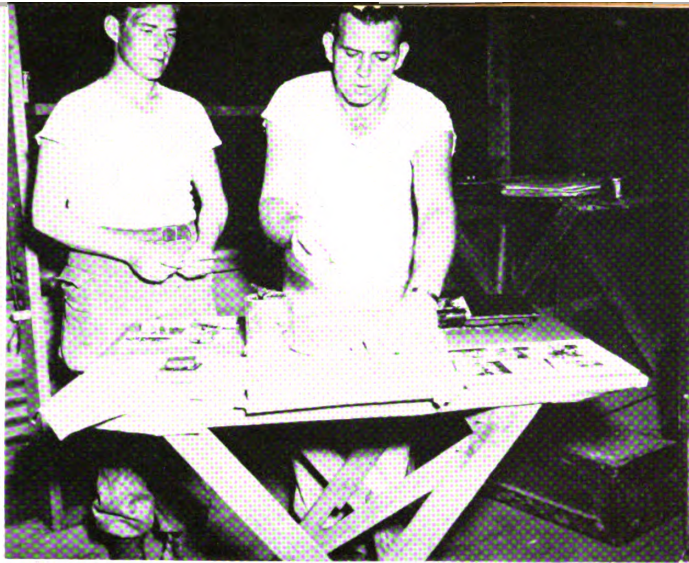




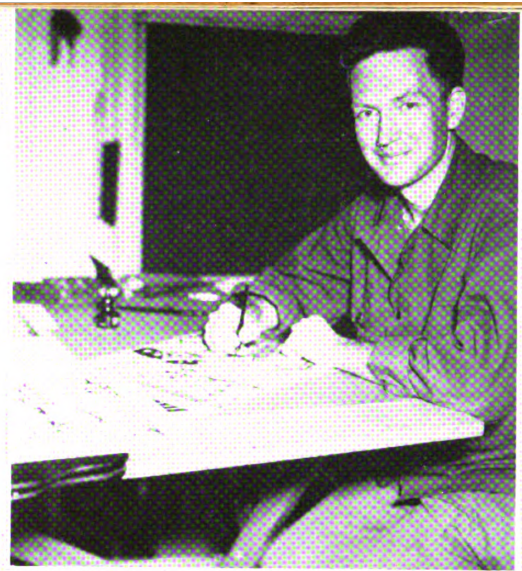
FULL FIELD INSPECTION - H/S CO.



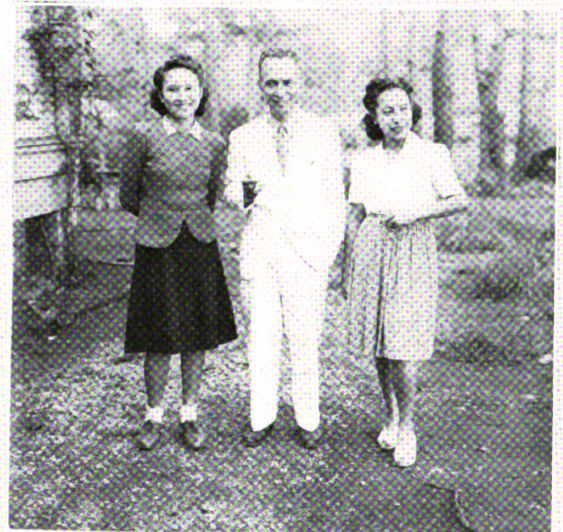
Jap Souvenirs—Nobody took the tank though.



Haslup, Welty and Hayes working on the album



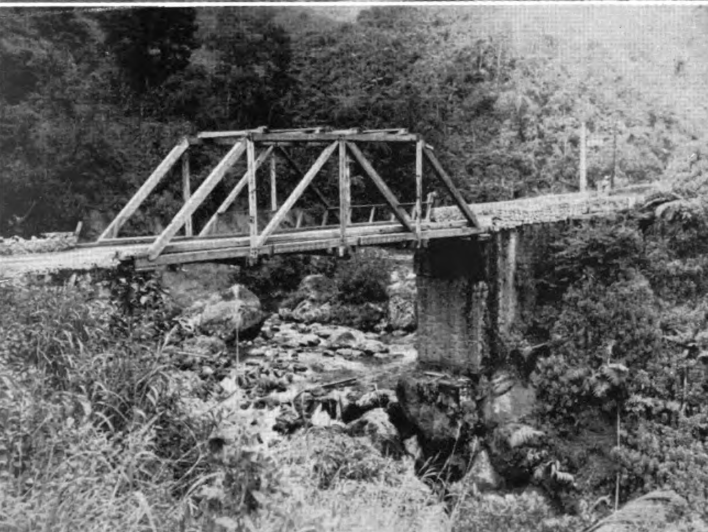
Ralph Leo, The Battalion Photographer



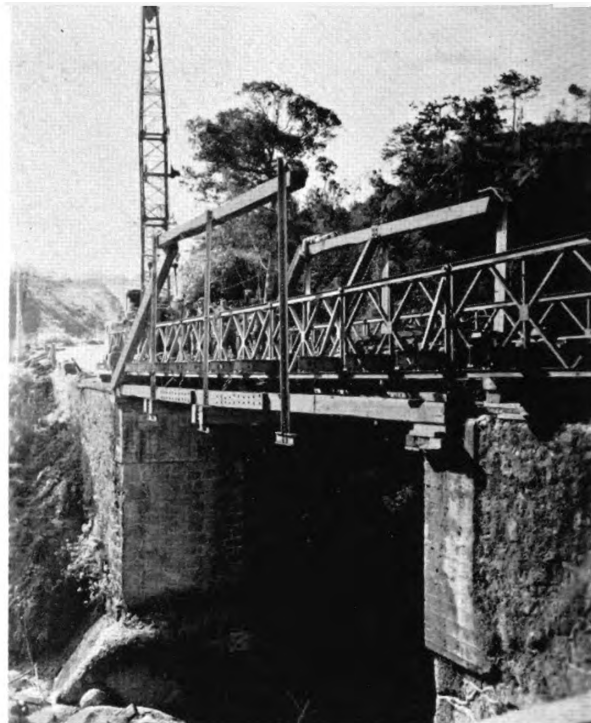
Doctor Manzanilla and daughters



Baguio's best Hash House
UNIVERSITY OF MICHIGAN



Baguio General Hospital.



IRISAN RIVER BRIDGE

The Irisan River gorge near Baguio on Route 9 was first spanned with a Bailey bridge by engineers of the 37th Division during their advance on Baguio. This bridge was later replaced by the 340th Engineers using a Queenpost truss, pre-fabricated type wooden structure.

The Bailey bridge was stripped of the inside panels and left in place as support for construction of the Queen-post truss. The truss bridge was erected inside of the old Bailey bridge, which was removed after the truss was in place. One of the most aggravating problems was the maintenance of a by-pass through the rocky gulch. The job was done by Company "B", and some cleanup work by "C" Company.



CORPS OF ENGINEERS

There's a song about the Army
The Navy, and the Marines
They've got one for the Air Corps
The whole darn works, it seems.

But the Engineers aren't kicking,
For when the Army comes in,
We know it's just another place
Where we've already been.

But they've never taken trouble
Tho' we've served them all these years,
To even write a verse or two
For the Corps of Engineers.

Before the Army got there,
We had to break the ground
And built it all to suit their needs,
Solid, safe and sound.

We've built their roads and airfields,
Their pipelines and their camps,
From underground munitions dumps,
To concrete loading ramps.

"If the Army and the Navy
Ever look on Heaven's scenes,
They'll find the streets are guarded
By the United States Marines."

Railroads, dams, and bridges,
Electric high power lines,
Canals, and docks and harbors
Even coal and iron mines.

Now who will guard the streets up there
We aren't disposed to say;
But we offer this suggestion
If they look at things this way.

When the Marines have taken over
In the land that knows no years
We think they'll find it all designed
By the Corps of Engineers.

By "Anon" Co. B

COMPANY A



HEADQUARTERS PLATOON

First Row—Left to Right: Bachul, Martini, Nix, Giglio, Beard, York, Walker, Dahlin, Hesemann. **Second Row—**O'Fallon, Hofer, Givens, Rekola, Flora, Loudon, Vickaryous, Broten, Sabbath, Landrenau, Schneider. **Third Row—**Zaya, Zimmerman, Rognlie, Jordan, Gomez, Ferguson, Park, Sponseller, Chiles, Lazowski. **Fourth Row—**Neitsch, Bruns, Garner, Owens, Lynch, Abraham, Gammon, Meyer, Wilhelm, Becker, Gillespie, G., Tringale, Kirby, Kehrner, Peterson, C., Hussen, Mejaski.



1ST PLATOON

Front Row—Left to Right: Fornari, Szot, Marquis, Bauer, Hopkins, Galetka, Daugherty, Meadows, Hindman. **Second Row—**Adkins, Aunchman, Moore, Olstad, Rucki, Larson, Marshall, Slaninka, Ezell, Murphy. **Third Row—**Mosher, Bellet, Peffley, Gotham, Dove, Pederson, Bannister, Britton, Anderson, L., Hanamann, Schroeder.



2ND PLATOON

Front Row—Left to Right: Boxrz, Beiter, Pradmore, Nevala, Lorenz, Pomella, Jones, Gibbs, Lloyd. **Second Row—**Manenti, Mahoney, Smith, R., Oertel, Hill, Hall, Adams, Maynard, Burns, Gillespie, R. **Third Row—**Grosteffon, Oltman, Meaney, Sexstone, Standal, Whittington, Anderson, T., Cuff, Ruble, Hoeft, Detert, Mazzetti.



3RD PLATOON

Front Row—Left to Right: Hansen, Kiskadden, Spackman, Anseline, Tillis, Kendrick, Mobley, Marston, Charmello. **Second Row—**Kavulich, Osborne, Lathem, Rowe, Gurgiolo, Shroyer, Madigan, Bailey, Cartie. **Third Row—**Tarro, Gahn, Malliott, Whitaker, Majeski, King, Mills, Sterritt, Jacobson. **Fourth Row—**Fulghum, McDaris, Loague, Romines, Jensen, Clark, Hanson, Peterson, D., Hamilton.

COMPANY B



HEADQUARTERS PLATOON

Front Row—Left to Right: Wohlwend, Sydor, Ruble, Czacherski, Dillon, Fugate, Smith, A. J., Amancio, Gillespie.
Second Row—Torgerson, Tutt, G., Smith, L. T., Sanders, Knutson, Varner, Carrell, L., Bonan, Del Grecco, Vargo.
Third Row—Griffith, Hodson, Sumpter, Wilson, Roy, Frederick, Macari, Bachman, Carroll, Chas., Klimkiewicz, Totsky, Fuentes, Schroeder.
Fourth Row—Singleary, Miller, Morris, Sego, Driskell, Stromberg, Dostatni, Aleo, Bregar.



1ST PLATOON

Front Row—Left to Right: Cutaro, Bilek, Humill, Canale, Reiss, Noble, Teruh, Draegert.
Second Row—Stewart, Chas., Dolm, Hanchuruck, McGlone, Amaro, Ortego, Romanoski, Trimm, Calderon.
Third Row—Fischer, Wertman, Martin, Niemi, Harvell, Melious, Holt, Wright, Alvin, Tutt, N.
Fourth Row—Palka, Burkett, Julian, Wooten, Sell, Drews, Carpenter, Benson, Ondrusek, Pontillo.



2ND PLATOON

Front Row—Left to Right: Wingo, Ciccone, Jaramillo, Hrsinko, Goodrow, Hickey, Delaney, Rolston.
Second Row—Tabin, Vinyard, Flowers, Juopperi, Didjunas, Blankenship, Oursland, Colley, McGarvey.
Third Row—Bastalich, Swanson, Farmer, Porter, Cornett, Flood, Conradi, McLaughlin, Ragazinski, Pristave, Fallon.
Fourth Row—Stewart, A. L., Menninga, Howard, Colarito, Fulkerson, Rohl, Struss, Osa.



3RD PLATOON

Front Row—Left to Right: Barros, Schwartfigure, Krafthefer, Royer, Amundson, Kane, Hoffman, Ridgeway, Barker.
Second Row—Gault, Hauptman, Briddick, Ferebee, Swisher, Sawyer, McDermott, Hammond, Fahrback, Milbrandt.
Third Row—Davidson, Roper, Navyac, Samson, James, Showers, Hadderson, Maloney, Beadle, Culp, Anderson.
Fourth Row—Smith, M. J., Ansdemore, Wright, D., McNamee, Shaffer, West, McCleney, Ashorn, Mistura, Laharty.

COMPANY C



HEADQUARTERS PLATOON

Front Row—Left to Right: Pirollo, Hamilton, Strader, Gonzales, Harman, Hall. **Second Row—**Guentensberger, P., Napier, Kraft, McDonald, Liszka, Russ. **Third Row—**May, Reeday, Lytle, Liszka, J., Schoessel, Klebold, Lopez. **Fourth Row—**Mizera, Urtel, Litchford, Wells, Whitlock, Quintavalle, Wilson, Pieper. **Fifth Row—**Stafford, Chaffin, Mee, Williams, H., Hauserman, Carlson. **Sixth Row—**Stine, Guentensberger, Morton, Wynkoop, Schriber, Lenzen, Reihl. **Seventh Row—**Rumerfield, Kirstein, Glogowski, Stuck, Fieker, Sumski, Williams, E.



1ST PLATOON

Front Row—Left to Right: Lewis, Cole, Jergenson, Wadsack, Cullumber, Campbell. **Second Row—**Jenniges, Miller, Brady, Berra, Hoftender, McClurg. **Third Row—**Wiggins, O'Reilly, Harless, Hahlweg, Silbough, Jablonski. **Fourth Row—**Malone, Develski



2ND PLATOON

Front Row—Left to Right: Boney, Luedecke, Fedor, Eather-ton, Leonard, Fiora, Hollingshead. **Second Row—**Szymakowski, Latko, Sliva, Grasmick, Manly, Kallunki. **Third Row—**Gunia, Lauman, Schon, Wildes, Stephens, Herrington. **Fourth Row—**Starnes, Stratz, McCullar, Warren, Gill, Warning, Staberl. **Fifth Row—**Skillern, Crites, Reitmeier, Zaczycki, Dahl, Lowe. **Sixth Row—**Davis, Crow.



3RD PLATOON

Front Row—Left to Right: Simmons, Siskar, Kreusler, Gambino, Toth, Urban, Jennemann. **Second Row—**Mueller, Hennessy, Schmid, Martinez, Grassmyer, Buskirk, Pacitto. **Third Row—**Hollinger, Morkert, Rinehart, Spears, Myers, Stirk, Collins. **Fourth Row—**Johnson, Mathews, Jarvis, Nein, Brucker, Wiese, Christopher. **Fifth Row—**Laufenburger, House, Springsteen, Budde, Menssen, Fleck, Tawes. **Sixth Row—**Christensen, Lander, Medrano, Kolm, Meredith, Meisenheimer.

H/S COMPANY



COMPANY HEADQUARTERS & ADM. & BSO.

Front Row—Left to Right: Remack, Capozzi, Shea, Kossman, Jimmerson, Gwiazdowski. **Second Row—**Norton, Jackson, McCabe, Thornberry, Maher. **Third Row—**Lopez, Lett, Gregory, Scott, Stewart, Adolphson. **Fourth Row—**Peterson, Leslie, Marshall, Tierney, Cannon, Klopelok. **Fifth Row—**Riedl, Biederman, Bower, Gardner, Breidert, Straub.



TRANSPORTATION SECTION

Front Row—Left to Right: Yates, Green, R., Bolduc, Belis, Banks, Pishnery. **Second Row—**Crawford, T. M., Roberts, W. F., Thomas, J., Reese, Hill, Watson. **Third Row—**Kuykendall, Shell, Miller, C., Johnson, FE., Levan. **Fourth Row—**Maloolley, Marking, Hibbs, Gordon, Richlovsky. **Fifth Row—**Wirt, Prestridge, Boyd, W., Adams, Bloom.



REPAIR SECTION

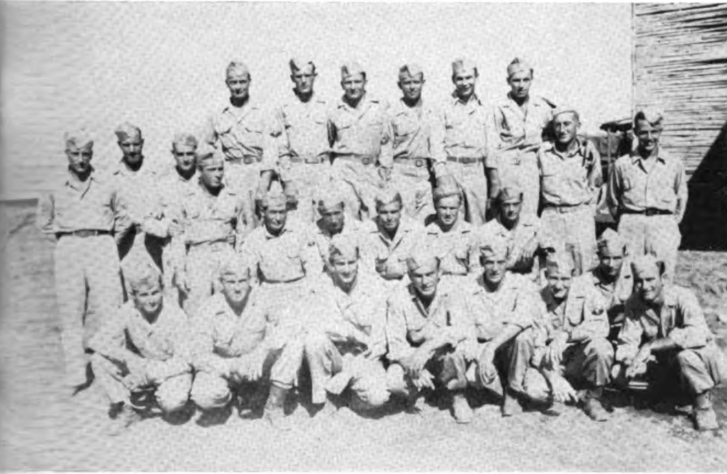
Front Row—Left to Right: Johnson, CD., Angotti, Parker, J., Zito, Weide. **Second Row—**Foley, Pesko, Siedow, Olshove, Schulman. **Third Row—**Wychor, Schulte, Wolner, Herring, Daniels. **Fourth Row—**Cesinger, Morgan, G., Boyd, O., Okragly, Childers, Long.



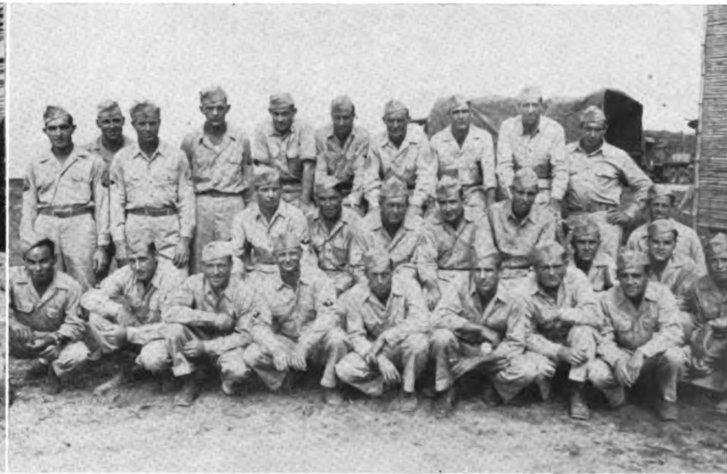
S-3 ENGINEER SECTION

Front Row—Left to Right: Ready, Fraga, Eldredge, Petish, Carson, Ballard. **Second Row—**Gitschlag, Hayes, E., Mac Donald, Levine, Steger. **Third Row—**Haslup, Hayes, H., Humes, Nessmith, Lippert. **Fourth Row—**Appleton, Raspel, Price, Novack. **Fifth Row—**Shaffer, Neprud, Crawford, J.

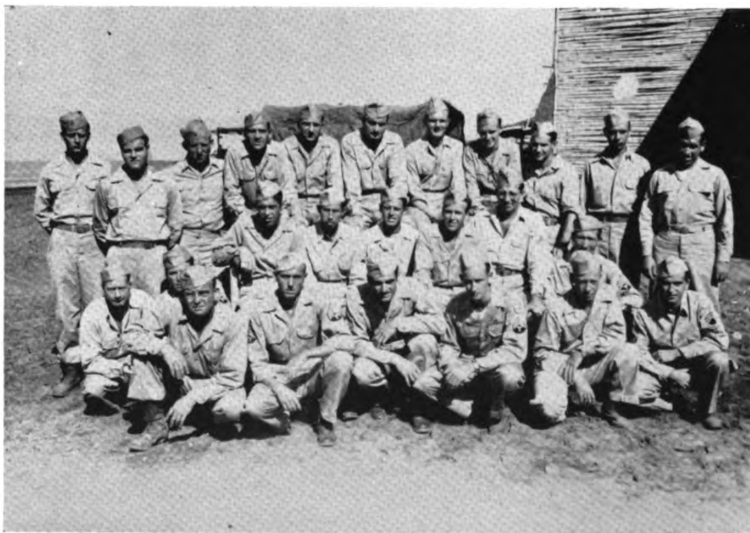
H/S COMPANY



Bottom Row—Left to Right: Kloss, Cruse, Romaine, Klingbeil, Yancey, Byrd, Davidson, Cox. **Second Row—Seated:** Jordon, Mohar, Mellon, Penatzer, Henke. **Third Row—Standing:** Hansen, Bergvig, Ladd, Robertson, Blake, Hawkins, Miller, Shahan, Couvillion, Parisi, Alm, Blazer.



Bottom Row—Left to Right: Leon, Huber, Grisham, Woodward, Crites, Davis, Rosenbaum, Floetke, Gilb. **Second Row—Seated:** Szabo, Warren, Brugman, Smith, Jones, Blake, Coleman, Karau. **Third Row—Standing:** Tibke, Rittenour, Fredrickson, Christianson, Patient, Shacklette, Hinrichs, Hampton, Roberts, Barron.



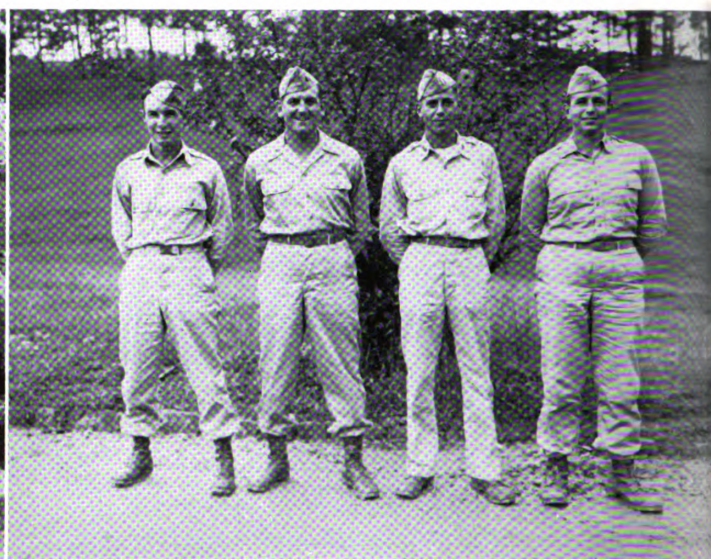
Bottom Row—Left to Right: Jones, Gilb, Page, Despain, Greene, Keller, Nimrod, Thayer. **Second Row—Seated:** Barry, Lavato, Karau, Marolt, Ebling, Mooney. **Third Row—Standing:** Adair, Coleman, Crawford, Floetke, Skrob, Raffin, Scherba, Brindamour, Parker, Terrill, Herrera.

OFFICERS



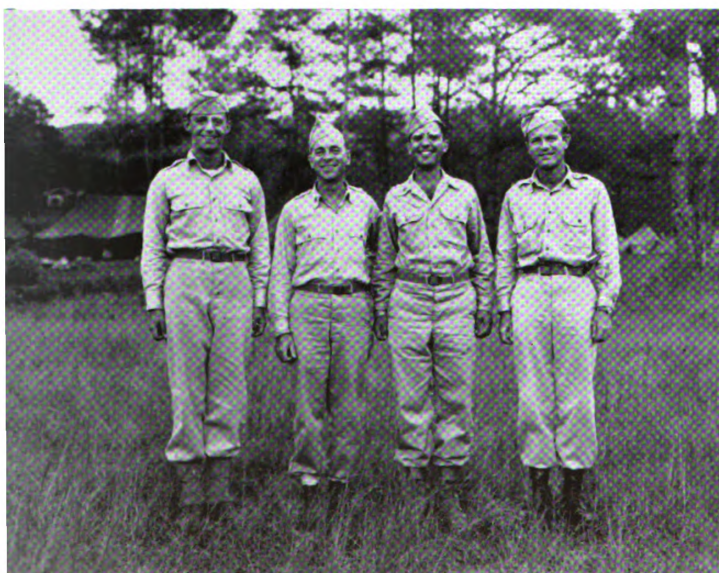
STAFF

1st Row: Schumacher, Robertson, Roos.
Back Row: Greene, Crumrine, Corbett



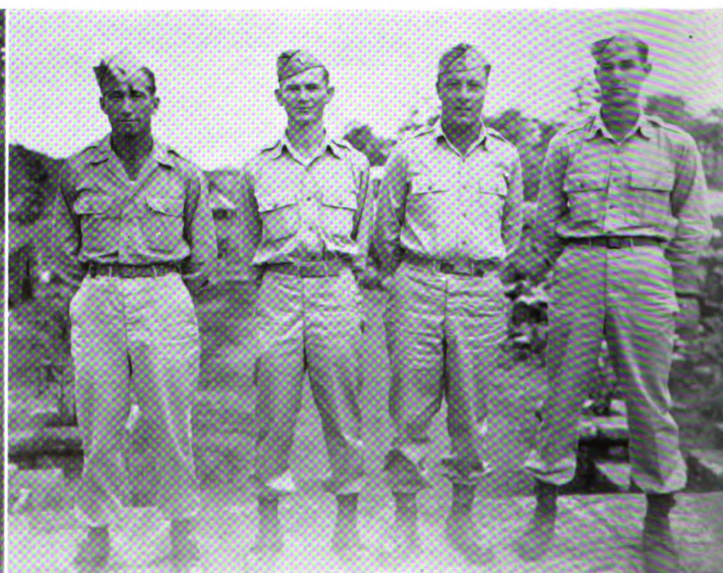
"A" COMPANY

Left to right: Crumrine, Anderson, Hilgendorf, Meuer.



"B" COMPANY

Left to right: Sarantopoulous, Slaybaugh, Mastriani,
Wimberly.



"C" COMPANY

Left to right: Carmody, Branstetter, Weinert, Schaeffer.



Chapel on the Beach.

PROPOSED TRIP TO JAPAN AND RETURN TO THE LOWLANDS

Early in October the unit was preparing for the move to Japan. Headquarters, "A" and "C" Companies were at Baguio; while "B" Company was stationed at Agoo preparing to handle loading operations. At that time there was little hope for the return to the United States of the 340th Engineers, even though seventy-five percent of the unit were high-point men. Equipment and supplies continued to pour in to B.S.O. in preparation for the coming movement to Japan. Some of the equipment was moved to the beach waiting to be loaded on LST's. By the middle of October the entire battalion assembled at Caba and at Agoo. The camp at Caba was situated on the beach of Lingayen Gulf. The men had received all the "shots" for plague and cholera necessary for the stay in Japan; and the unit was ready to move at a moment's notice.

Then the picture changed. Orders were received cancelling the move to Japan. B.S.O. began to turn supplies and equipment over to Base M and other engineer units. The outfit had received instructions to prepare for inactivation.

For two weeks the battalion lolled on the beach at Caba waiting for orders to go home. Many units were taking men with far less than 89 points back to the States. Members of the 340th continued to sweat-it-out. But finally late in October that great day came. On the 28th day of October half of the battalion was on its way to Manila, and the following day the rest of the high point men left on the first leg of the journey home.



They rolled their own.



"Got a light?"



Pounding & winnowing rice.



Shopping bags.



Fashionable hats.



Senior Class, High School, Agoo Luzon.



Farewell dance with the belles from Agoo.



Transportation to Manila reserved and awaiting orders.



340th Swimming Team.



Nurses going to Japan.



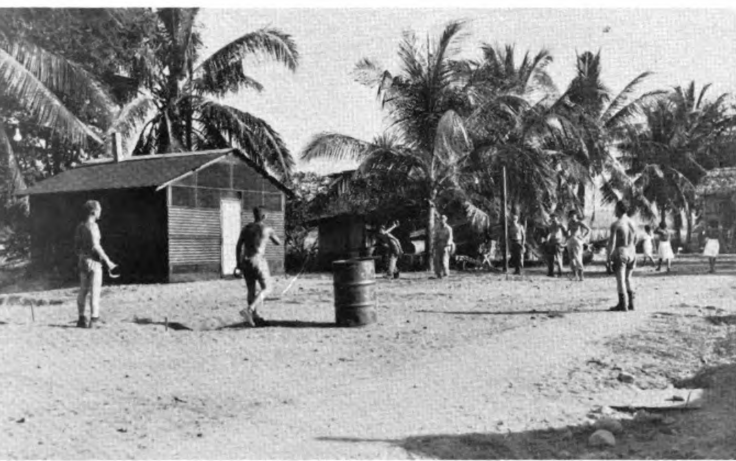
Chapel at old 92nd Evac Area at Caba.



"C" Company Softball team.



May hits a foul tip.



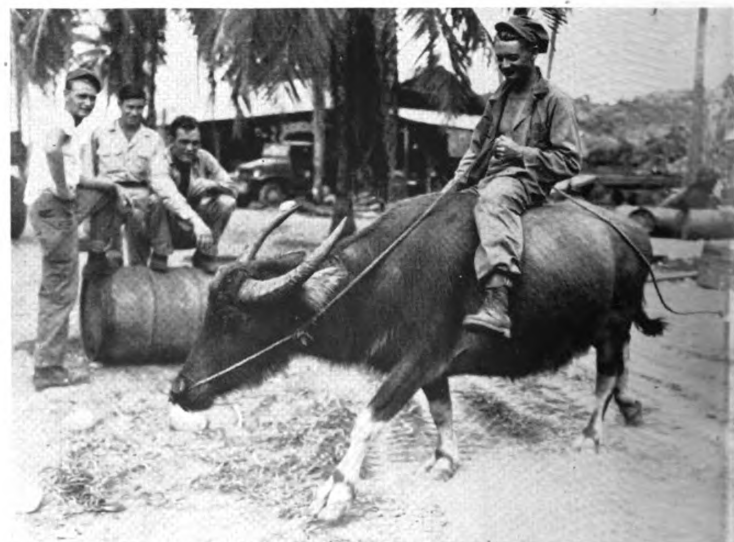
The farmers get their exercise.



Gaicho.



Left to right: Totzke & Duke.



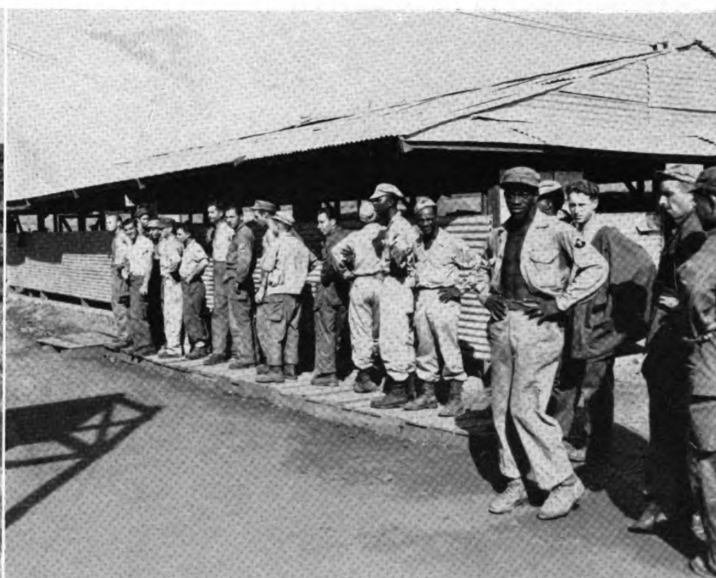
Ride 'em Shorty.



PROCESSING AT 21st "REPPLE DEPPE"



Chow lines were long here too.



Dollars for Pesos—A fair exchange.

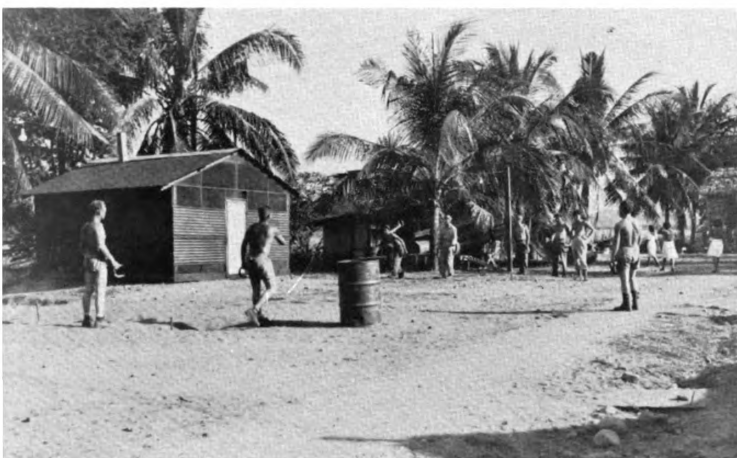




"C" Company Softball team.



May hits a foul tip.



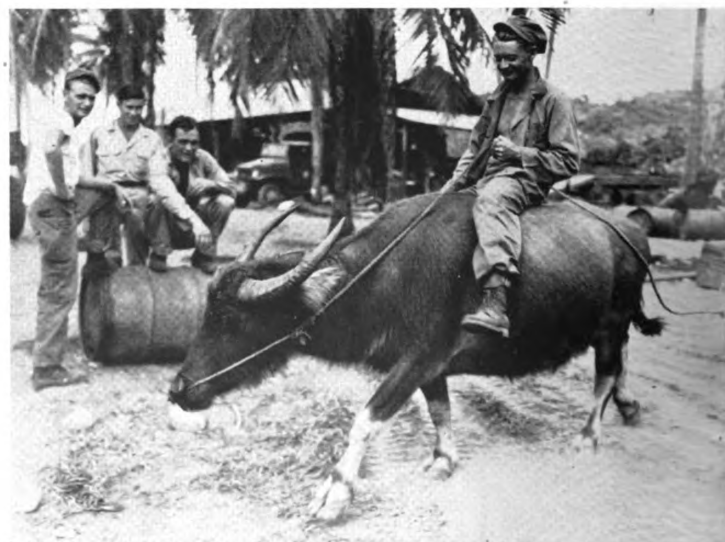
The farmers get their exercise.



Gaicho.



Left to right: Totzke & Duke.



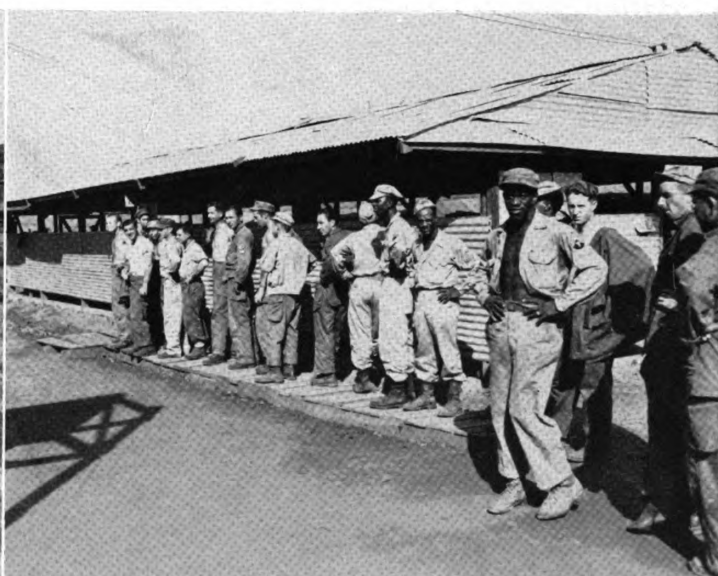
Ride 'em Shorty.



PROCESSING AT 21st "REPPLE DEPPLE"



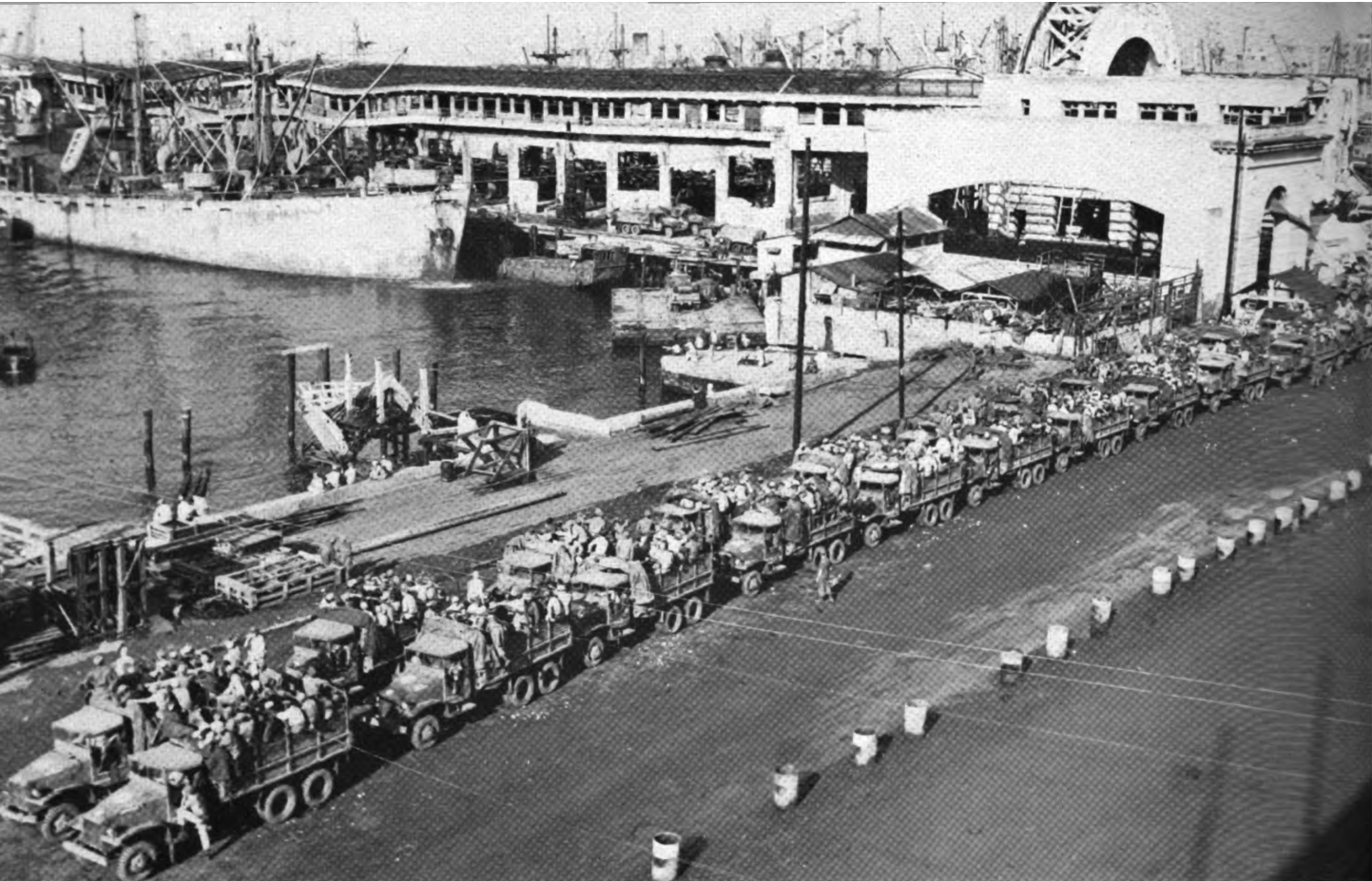
Chow lines were long here too.



Dollars for Pesos—A fair exchange.

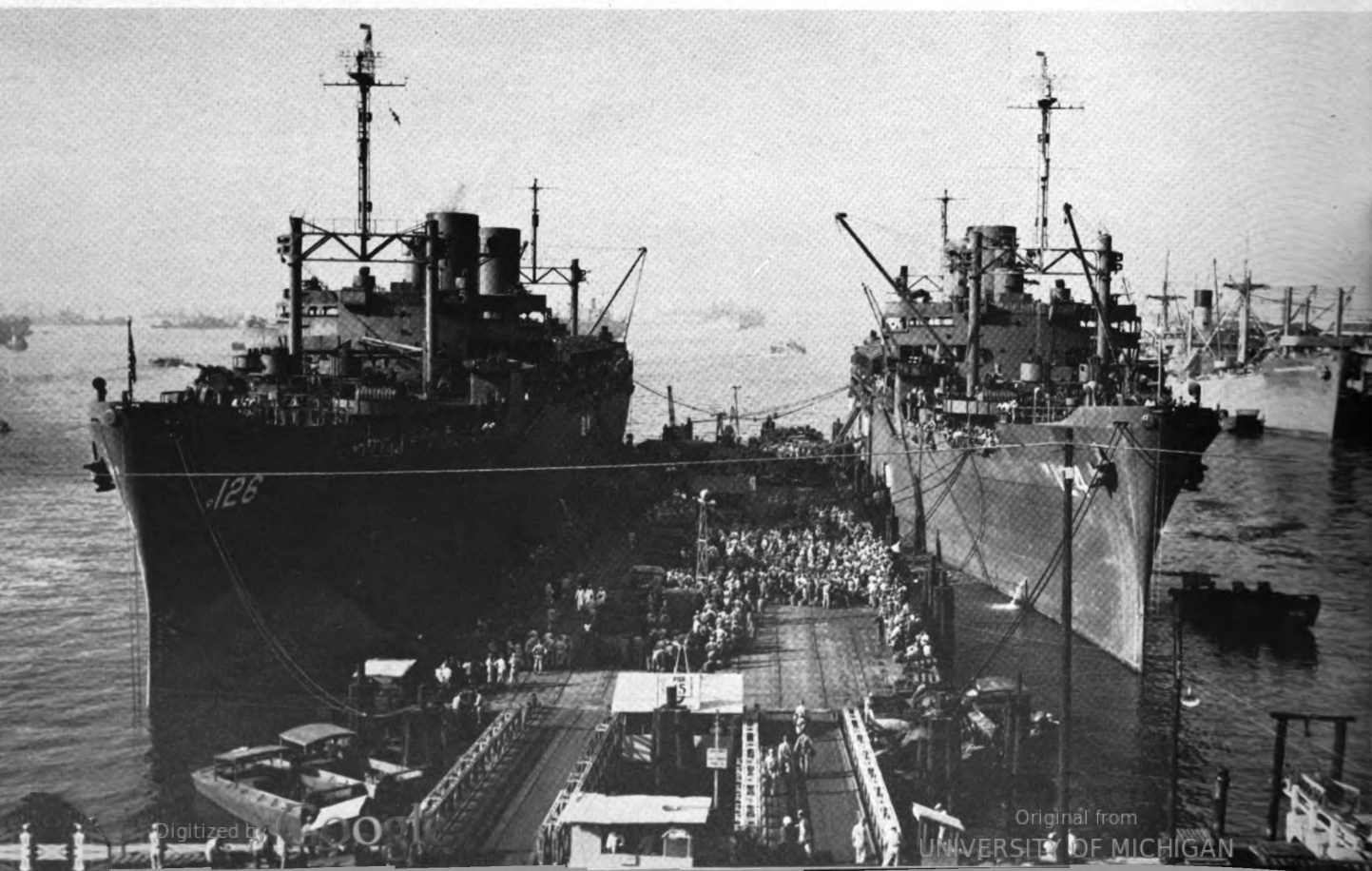


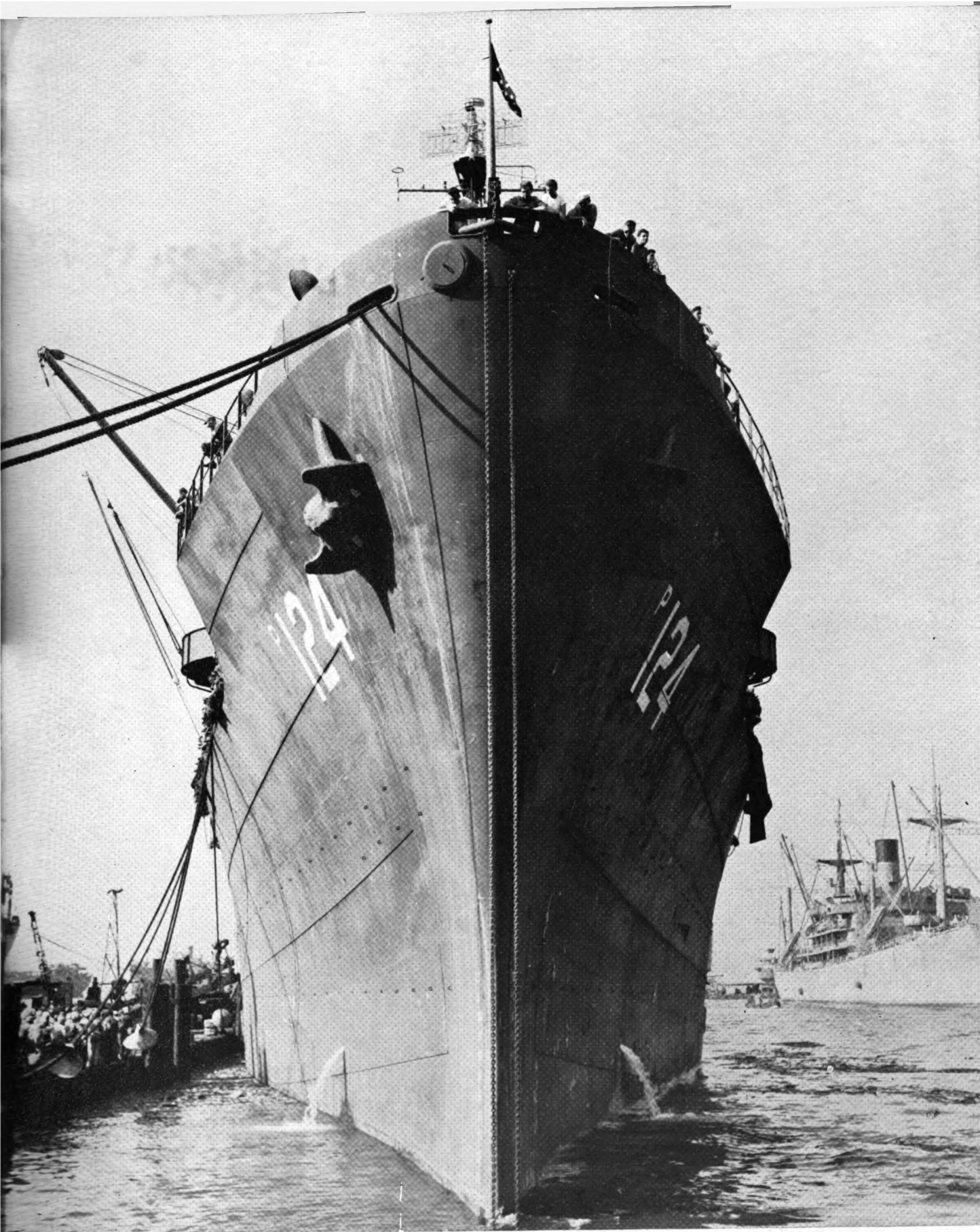
Waitin' for the ship to come in.



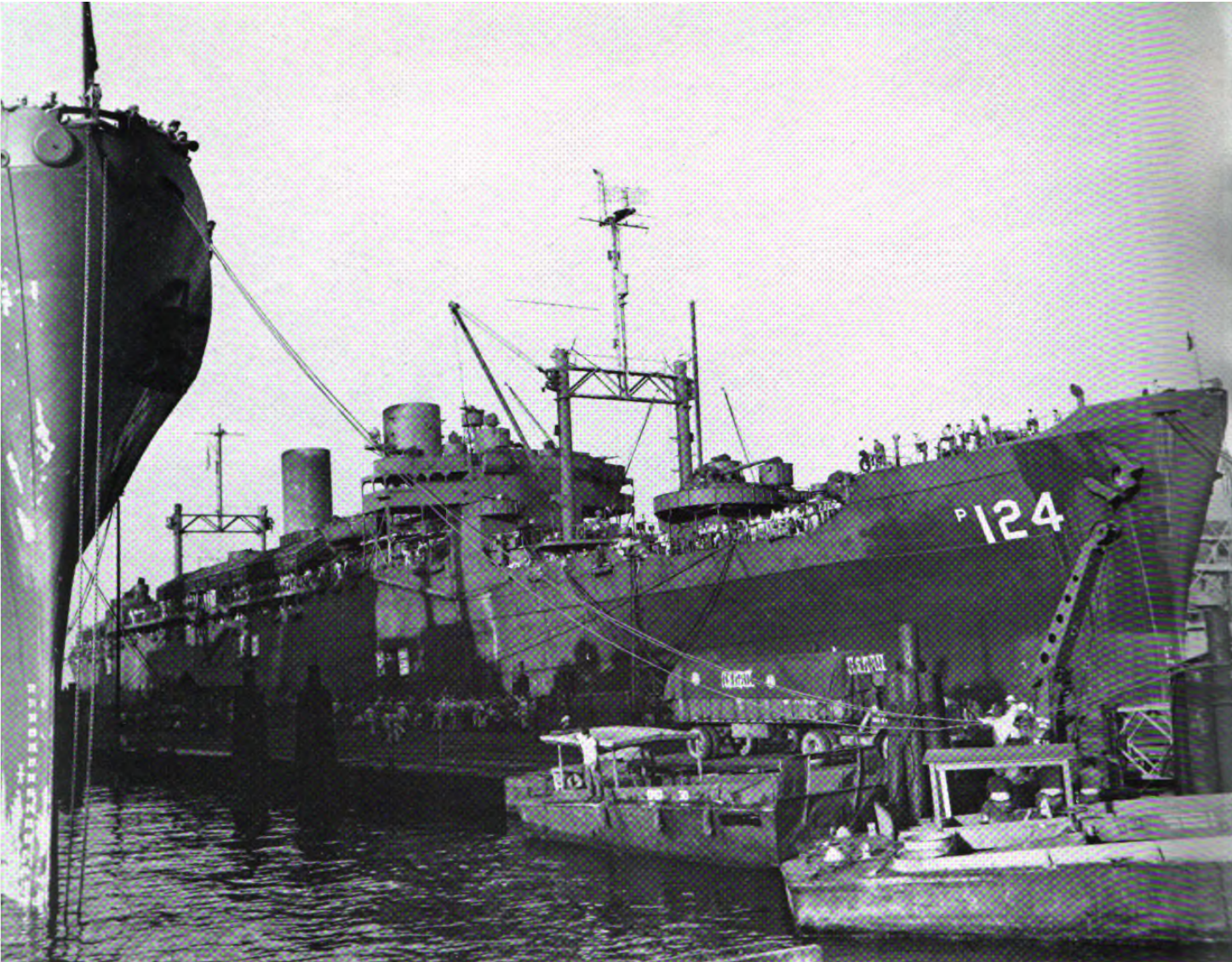
Troops arriving at Pier 15—

Admiral Sims and Admiral Hughes.





U.S.S. Admiral Hughes.



GOING HOME

A majority of the battalion—the high-point men who had been in the 340th since its Alaskan days—spent a dozen days at the Manila replacement depot. They spent their time in doing little more than nothing. All had their turn or two at KP, and from all appearances seemed to enjoy it. Upon entering the depot each man was processed for the trip home. A re-issue of clothing was made, P.X. supplies bought, and influenza shots taken as protection against the dangers of life in the States. The men were treated well during their stay at the "shipping company."

On November 9, 1945 some 500 members of the 340th Engineer Construction Battalion boarded the U. S. S. Admiral C. F. Hughes for the trip they had been waiting to take for almost four years. About 50 men went back on the sister ship, the Admiral Sims. This trip to the States and home, the seventh ocean voyage, was without a doubt the finest of all.

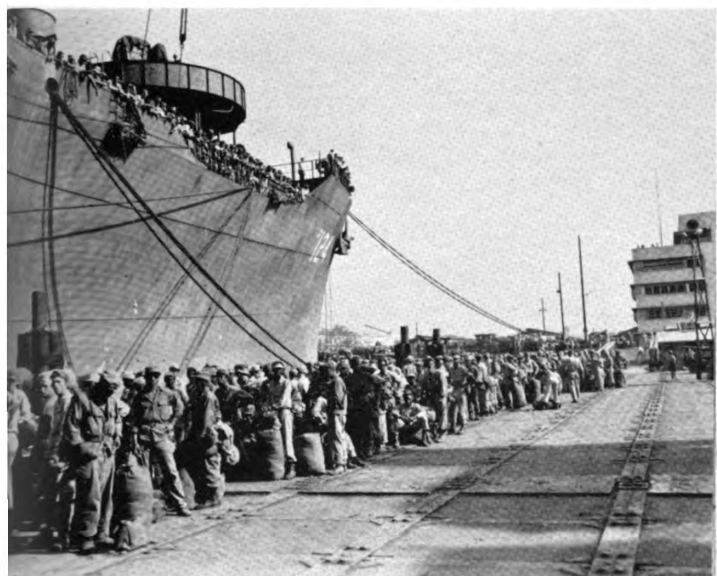
While aboard the men were kept quite busy, each having a definite job to do. The men on K.P. usually worked one day, with two days off. There were other jobs such as cooking, baking, cook's helper, and among other things, deck details. There was the usual entertainment: movies, programs of one kind or another, music, cards and "bull sessions." Most of the time was spent just talking about home and anxiously waiting for the first sight of America.

The crossing was made in sixteen days. On the way home things looked bad for awhile. It seemed as though the ship was going to fall apart; if it wasn't the screw, it was the rudder; and if it wasn't one or the other of these, it was the boiler. However, the Admiral Hughes moved toward the east, at top speed when all was right and at a snail's pace when the boiler, the rudder, or the screw was fouled up. But in spite of all the breakdowns, the ship arrived at San Francisco on the night of November 23, 1945, in better than average time. From Manila to Frisco via the "great circle route"—a distance of 6,462 miles.

On the morning of November 24, 1945, the men left the Admiral Hughes to board a river ferry which took them some 40 miles inland to Camp Stoneman, California. Here they were given a "Steak Dinner" with all the trimmings, including fresh milk. Since PW's were doing all the K.P., the men had nothing to do but wait. Phone calls and telegrams by the hundreds were sent to all corners of the country. In a few days the men began to move out on trains to the separation centers nearest their respective homes. And miraculously the red tape of separation processing was very short. So, by the middle of December, all of the Old Dogs of the 340th were shopping for Christmas presents, and getting ready for real celebration—AT HOME!



Manila Port Terminal and Base X Headquarters.

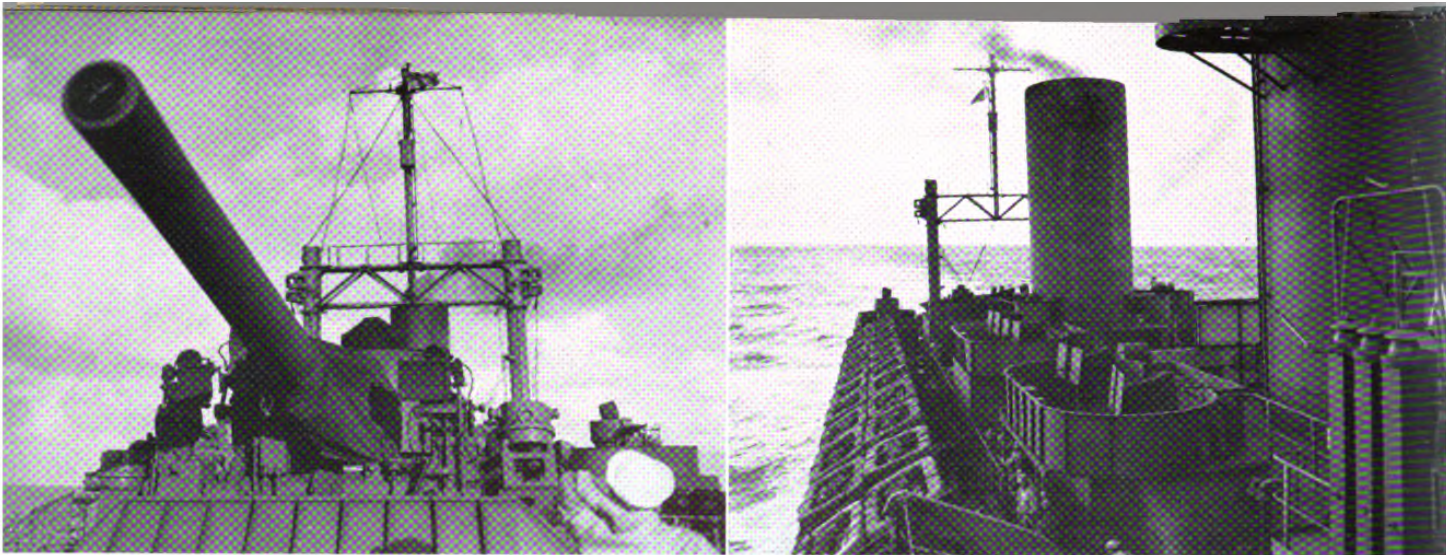


Lined up to board ship.



Up the gangplank.

Hauling up the gang plank.



Silenced guns.

Church services.





Now hear this,—
 "Army sweeping detail, man your brooms. Clean sweep down fore and aft; empty
 all G.I. cans and butt buckets.
 "Army cooks and KP's lay down to the mess deck.
 "The smoking lamp is lit topside."



Burial at sea.

SAN FRANCISCO







Camp Stoneman.



Wasatch Mountains in Utah.



Taking "five" on the homeward stretch.



Front Row, left to right: Joseph Near, Art Floetke, Ray Morrison.
 2nd Row: Charles Gitschlag, Dominic Tringale, Leo Battle, Stanley Mistura.
 3rd Row: Tom Palone, George Olshove.

RECONVERSION

Front Row, left to right: Floetke, Near, Morrison.
 2nd Row: Gitschlag, Mistura, Battle.
 3rd Row: Olshove, Palone, Tringale.



THE TOUGHEST OF THEM ALL

Three friends were sitting around a bar,
Each one smoking a black cigar,
Each one guzzling down a beer
Each one's eyes filled with fear.

Each had decided to go to war
To keep the Jap from his back door.
But each by some unearthly chance
Had joined a different branch.

The Marine arose on unsteady feet
His eyes were filled with much conceit
"When the war is over we'll meet again,
And I'll tell you stories of real fighting men."

The sailor smiled, "you will learn
when you hear of my return."
The Soldier didn't say a word,
And looked as though he hadn't heard.
"I'll neither brag nor boast, my men
Until I'm sure I'm back again."

The war was over and they were back
Drinking beer in the same old shack.
The Marine with ribbons on his chest
Rose to his feet before the rest.
"I saw action in the South Seas,
I shot down Japs right out of trees."

The Sailor rose with a great smile,
And laughed at the Marine for a while
"Friends, I really saw the fight,
In Italy, England and the Reich
I killed Germans to my delight
But you would lose your appetite,
If I told my every fight."

The Soldier didn't say a word,
And looked as though he hadn't heard,
Then he hit the bar and scratched his ears,
And said "I was with the 340th Engineers".

The Marine jumped up, the Sailor too,
"Brother! we owe the drinks to you"
For each had heard and knew too well
There sat a man returned from HELL!

By "Anon" (Poet Laureate
of Company "B")



MEN OF THE 340th

- WILLIAM ABBOTT
Blountstown, Florida
- LEO J. ABRAHAM
Route # 1
Clinton, Maine
- EARL H. ADAIR
1349 Summit Street
Columbus, Ohio
- CECIL W. ADAMS
1812 15th Ave., South
Minneapolis, Minn.
- WILLIAM J. ADAMS
6616 Van Dyke Street
Philadelphia, Penna.
- ROY A. ADKINS
2406 Maple Street
Columbus, Ohio
- NORTON W. ADOLPHSON
Luverne, Minn.
- HOMER ALDRICH, JR.
RFD # 2
Port Byron, New York
- ANTHONY ALEO
140 Chestnut Street
Everett, Mass.
- JAMIE H. ALEXANDER
127 S. Oak Street
Clarksburg, West Virginia
- JOHNNIE ALLEN
Fetty, Kentucky
- DALTON D. ALLISON
Route # 2
Waldo, Ark.
- BERNARD H. ALM
Hinkley, Minn.
- ABRAHAM M. ALOFF
83 Shore Drive
Winthrop, Mass.
- GEORGE M. AMANCIO
373 Liverpool Street
Pawtucket, R. I.
- ELIAS G. AMARO
P.O. Box 324
New Braunfels, Texas
- LESTER N. AMUNDSON
Becker, Minn.
- DARWIN M. ANDERSON
1008 S. 16th Street
Clarinda, Iowa
- ELMER F. ANDERSON
Box 35
Payne, Minn.
- KENNETH ANDERSON
Box 181
Watson, Minn.
- LEONARD F. ANDERSON
4923 Crystal Street
Chicago, Ill.
- OSCAR H. ANDERSON
408 West 4th Street
Bicknell, Ind.
- THEODORE ANDERSON
3129 Armitage
Chicago, Illinois
- VICTOR D. ANDERSON
4633 Cannon Avenue
Klamath Falls, Oregon
- THAD. ANGOTTI
Route # 1
Carruthersville, Missouri
- ROGER A. ANSELIN
1127 Speedway
Fairmont, West Virginia
- CURTIS L. APPLEGATE
Blum, Texas
- CHARLES W. ARBUTHNOT
Elkins, West Virginia
- CHARLES ARIX, JR.
Branchland, West Virginia
- PHILLIP R. ASEL
2745 S. Humboldt
Englewood, Colorado
- IRVIN ASHORN
Route # 1
Anderson, Texas
- PAUL H. ATWELL
Pearisburg, Virginia
- ANDREW J. AUNCHMAN
65 E. Street
Pawtucket, R. I.
- OTTO F. AUDEMORE
Route # 2
Crofton, Nebraska
- FREDRICK J. AUSTIN
670 Girard Avenue
Philadelphia, Pa.
- JOSEPH A. BACCI
736 W. Cornelia
Chicago, Ill.
- CHARLIE BACHMAN
Dumont, Star Route
Texas
- FRANK J. BACHUL
738 Quincy
Minneapolis, Minn.
- HUBERT E. BAILEY
203 E. North Street
Brookfield, Missouri
- MARION S. BAILEY
Box 527
Liberty, Texas
- GEORGE A. BALLARD
1424 S. Hanover St.
Baltimore, Maryland
- HAROLD Q. BALLEW
Copperhill, Tenn.
- WILLIAM L. BALLMAN
6719 Van Kirk Avenue
Carthage, Ohio
- CHARLES BANEY
Sta. A.—Box 102
Clarksburg, West Virginia
- JOE H. BANKS, JR.
543 Buckingham Street
Columbus, Ohio
- MACK W. BANNISTER
313 Harvey Street
Borger, Texas
- EDWARD BARANOWSKI
2940 Yemond St.
Hamtramck, Mich.
- DANIEL BARKER, JR.
709 N. Bell Street
Kokomo, Ind.
- ODELL P. BARRON
Route # 3
Cumming, Georgia
- WALTER BARROS
639 Wickender Street
Providence, R. I.
- ALFRED BARTOLINO
Benld, Ill.
- WILLIAM S. BASTALICH
815 Railroad Avenue
Rock Springs, Wyoming
- WILBERT F. BASTIAN
Route # 1, Box 66
Coleha:bor, N. Dakota
- ERNEST E. BAUER
216 S. 8th Street
Neblesville, Ind.
- ALLAN BEADLE
122 Ea:1 14th Street
Columbia, Tenn.
- HARRY J. BEARD
Route # 1
Carlos, Indiana
- EMERSON G. BECKER
214 N. Van Eps
Madison, South Dakota
- ALBERT J. BEITER
138 East 8th Street
Clifton, N. J.
- ARTURO M. BELIS
1808 Adams Street
Artesia, California
- ELLSWORTH R. BELL
4431 Warner Road
Cleveland, Ohio
- RALPH L. BENNETT
Route # 2
Vinton, Ohio
- MILNER G. BENSON
707 W. Maple
Stillwater, Minn.
- RAYMOND BERGVIG
Box 524
Pine City, Minn.
- CHARLES F. BERRA
2331 Edwards Street
St. Louis, Missouri
- CECIL L. BERRY
Box 158
Candor, N. Carolina
- JOHN H. BICKEL, JR.
539 S. Western Pkwy.
Louisville, Kentucky
- MURVYN L. BIEDERMAN
Box 61
Roundhead, Ohio
- ALOIS V. BILEK
209 Ash Avenue South
Montgomery, Minn.
- CLARENCE W. BILLINGS
Route # 1
Covington, Tenn.
- JAMES C. BLACKWOOD
1602 Hoefgen Avenue
San Antonio, Texas
- HERBERT C. BLAKE
Route # 2
Moundsville, West Virginia
- ROY P. BLAKE
Candor, North Carolina
- ROSARIO BLANCHETTE
27 Union Street
South Bridge, Mass.
- ROBERT M. BLAND
Greenup, Ill.
- HUBERT F. BLANK
420 W. McCarty Street
Jefferson City, Mo.
- DON BLANKENSHIP
Dean, Tenn.
- LAWRENCE A. BLASING
307 Church St.
Lake Mills, Wisconsin
- PAUL G. BLAZER, JR.
1412 Bath Ave.
Ashland, Kentucky
- JOHN E. BLOCK
Route # 1, Box 48
Kermit, West Virginia
- JAMES BLOODWORTH
Box 629
Clarksville, Texas
- THOMAS X. BLOOM
Slanesville, West Virginia
- PATRICK K. BOLDUC
RFD # 4
Laconia, New Hampshire
- ALLEN F. BOLLIN
3222 N. Leavitt Street
Chicago, Illinois
- MICHAEL BONAN
215 W. 6th Street
Leadville, Colo.
- JOSEPH V. BORG
4428 2nd Blvd.
Detroit, Mich.
- JOHN D. BOWER
655 S. Ohio Avenue
Columbus, Ohio
- EMILE F. BOXRZ
2818 E. Thomas Street
Seattle, Wash.
- OWEN BOYD
2023 E. 7th Street
Pine Bluff, Ark.
- WILLIAM C. BOYD
839 S. Agnes Avenue
Dayton, Ohio
- JAMES E. BRADY
1045 Wall Street
St. Louis, Missouri
- EDGAR G. BRANSTETTER
109 Duck Street
Stillwater, Okla.
- HERBERT BRECHTL
Plain, Wisconsin
- RAYMOND J. BREGAR
1120 N. Broadway
Joliet, Illinois
- ARTHUR E. BREIDERT
1036 laughborough Avenue
St. Louis, Missouri
- PHILLIP E. BRENNEN
205 Quincy Street
Twin Falls, Idaho
- FRANK M. BREUNIG
Route # 1, Box 631
Arlington, Calif.
- CARL J. BREZINA
2993 E. 82nd Street
Cleveland, Ohio
- JOHN E. BRIDICK
Route # 2
Morris, Illinois
- LEROY BRIDGES
223 West Oklahoma
Knoxville, Tenn.
- ROBERT L. BRINDAMOUR
8 Euclid Avenue
Bradford, Mass.
- LLOYD A. BRITTON
4729 Cote Brillante
St. Louis, Mo.
- JOHN B. PROBST
1605 Hampton Blvd.
Reading, Pa.
- FRANCOIS S. BRODEUR
658 Rogers Avenue
Brooklyn, New York
- ROBERT BROOKS
1319 25th St., N.W.
Canton, Ohio
- EMMETT A. BROTEN
6308 Ogden Avenue
Superior, Wis.
- RUSSELL E. BRUCKER
Paris Road
Sauquoit, N. Y.
- STEPHEN W. BRUGMAN
RFD # 1
Nichols, Iowa
- FRANCIS L. BRUMANA
630 S. Lincoln Street
Santa Maria, Calif.
- GALEN C. BRUNS
410 Park Street
Elgin, Illinois
- ANTON P. BUDE
Bortelso, Ill.
- JAMES B. BURDETT
18 Mountains St.
Whitehall, N. Y.
- HAROLD G. BURDOCK
Route # 1
Portage, Ohio
- ISOM S. BURKETT
Kenedy, Texas
- HAROLD F. BURNS
301 N. Green St.
Anna, Illinois
- JACK D. BUSCHMANN
1247 Marion Drive
Glendale 5, Calif.
- PAUL F. BUSKIRK
Box 124
Cambria, Virginia
- ARTHUR BUSKY
Brushton, N. Y.
- MAURICE L. BYRD
Salem, South Dakota
- JOHN CAFARO
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Grove, N. J.
- IGNATIUS CALDERON
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Detroit, Mich.
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702 E. Arch Street
Jerseyville, Ill.
- THOMAS L. CANALE
408 S. Franklin
Dwight, Illinois
- JOHN J. CANNON
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Elmhurst, N. Y.
- THOMAS W. CANTOR
826 S. Michigan Avenue
Wellston, Ohio
- JAMES A. CAPOZZI
390 Myrtle Avenue
Brooklyn, N. Y.
- NEIL A. CARLSON
Humluplups, Wash.
- THOMAS J. CARMODY
71 Kinfield St.
Providence, R. I.
- EDWARD A. CARPENTER
Chesterstown, N. Y.
- LEO CARRELL
Greenup, Ill.
- CHARLES CARROLL
Minns Ave., Rt. 4
Patterson, N. J.
- CARLTON E. CARSON
RFD # 3
Bayne City, Mich.
- HILARY R. CARTIE
Kelsey, Minn.
- ALFRED R. CESINGER
Route # 4, Box 569
Terre Haute, Ind.
- BEN R. CHAFFIN
Route # 3
Cookeville, Tenn.
- WILLIAM E. CHAPPLE
118 Clayton Street
Macon, Georgia
- ANTHONY J. CHARMELLO
324 Henu Street
South Amboy, N. J.
- RALPH W. CHASE
408—4th St.
Ironton, Minn.
- DAVID C. CHILDERS
1017 Monroe Falls Ave.
Cuiahoga Falls, Ohio
- WILLIAM R. CHILES
Makane, Mo.
- ALBERT E. CHRISTENSEN
62 Crest Avenue
Revere 51, Mass.
- CARL J. CHRISTENSEN
Bryant, South Dakota
- JOSEPH F. CHRISTIANSON
Everest, Kansas
- WILLIAM R. CHRISTOPHER
Route # 1
College Grove, Tenn.
- NICHOLAS CHWAT
120 S. 1st St.
Brooklyn, N. Y.
- MICHAEL CICCONE
151 Brookdale Ave.
Newark, New Jersey
- EDWARD E. CLARK
6123 Chestnut
Kansas City, Mo.

CARL J. CLEMENS
1276 James Avenue
St. Paul, Minn.

JOSEPH D. CLINE
Route # 3
Vandalia, Ill.

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Cross Plains, Texas

ROBERT D. COFFMAN
420 E. Maynard Avenue
Columbus, Ohio

ANTHONY COLAVITO
286 Edmund Ave.
Patterson, N. J.

GEORGE B. COLE
c/o Mrs. John Krug
2627 W. Farwell Avenue
Chicago, Ill.

MILTON C. COLEMAN
5600 Wababa
St. Louis, Missouri

EDWOOD W. COLLEY
Route # 3
Monroe, Maine

LAWRENCE A. COLLINS
Box 503
St. Cloud, Minn.

ALFERT J. CONRADI
Route # 2
Syracuse, Nebr.

JAMES M. CORBETT
Santa Clara University
Santa Clara, Calif.

JOSEPH E. CORNETT
Route # 1
Afton, Texas

HARRY P. COUVILLON
Moreauville, La.

LEONARD F. COX
2449 N. Perdum Street
Kokomo, Ind.

JOHN W. CRAWFORD
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NOTE: In this book the photographs, maps, cartoons, descriptions, and designs were composed and executed by men of the 340th Engineers. It has been the purpose of the men who worked on this last project of the unit to fashion, by careful workmanship, a worthy monument to men of the 340th, and that all who read it will thereafter respect fully the accomplishments of the 340th Engineers and all U.S. Army Engineer troops "In the Pacific".

Only a limited edition of the book was printed, according to the number of subscriptions paid before publication. There are a few extra copies available to former members of the 340th, but none can be sold to the general public.

P.S.: As this book goes to press information has been received that the 340th Engineer Construction Battalion has been reactivated as a Regular Army unit in the Pacific Theater. May it carry on the traditions of the original 340th.

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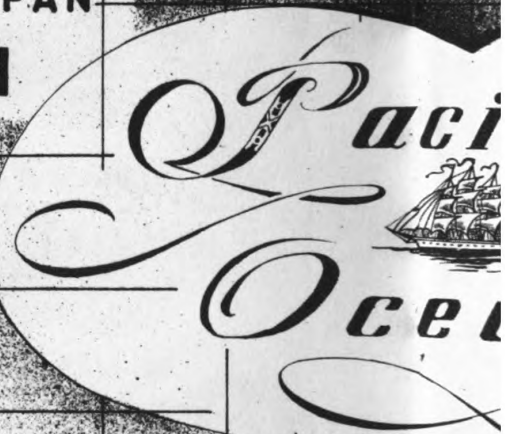


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